

300 - ROADWAYS

STREET FUNCTIONAL CLASSIFICATIONS

A. ARTERIAL

A GENERAL TERM DENOTING A HIGHWAY PRIMARILY FOR THROUGH TRAFFIC, CARRYING HEAVY LOADS AND LARGE VOLUMES OF TRAFFIC, USUALLY ON A CONTINUOUS ROUTE.

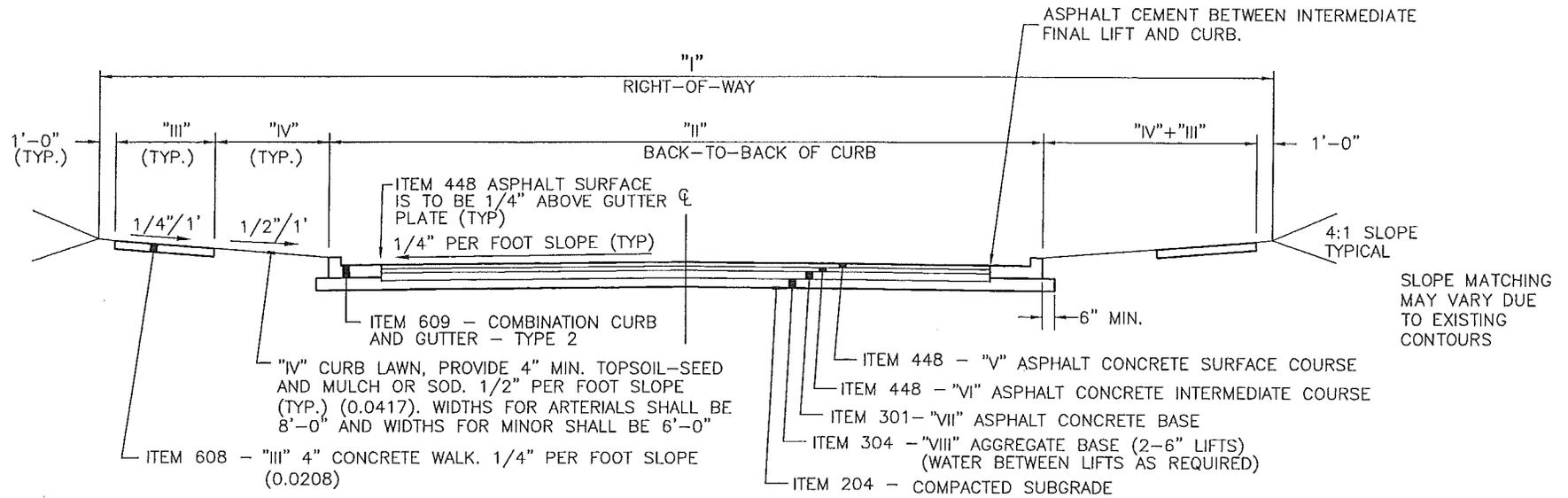
B. COLLECTOR

STREET DESIGNED TO CONDUCT TRAFFIC FROM LOCAL STREETS TO ARTERIALS. COLLECTOR HAS A FURTHER BREAKDOWN OF COLLECTOR-RESIDENTIAL AND COLLECTOR-INDUSTRIAL AND COMMERCIAL.

C. MINOR

A STREET DESIGNED TO PROVIDE ACCESS TO ABUTTING PROPERTY TO COLLECTORS.

DESIRED MINIMUM STANDARDS			
STREET FUNCTIONAL CLASSIFICATION	RIGHT-OF-WAY WIDTH	BACK-TO-BACK	BACK-TO-BACK CURB
	(L.F.)	(L.F.)	(L.F.)
ARTERIAL	80	59	59
COLLECTOR - RES.	60	37	41
COLLECTOR - IND. AND COMM.	60	37	41
MINOR	50	31	35



NOTES

- A. ALL WORK TO CONFORM TO ODOT CONSTRUCTION AND MATERIAL SPECIFICATIONS LATEST REVISION UNLESS OTHERWISE SPECIFIED.
- B. ITEM 407 TACK COAT, SHALL BE REQUIRED WHEN 10 DAYS HAVE ELAPSED BETWEEN ASPHALT PAVEMENT LIFTS UNLESS OTHERWISE SPECIFIED BY THE CITY. APPLICATION RATE IS 0.10 GALLON PER SQUARE YARD.
- C. ALL BUTT JOINTS SHALL BE SEALED WITH PG64-22 WITHIN 24 HOURS AFTER PLACEMENT OF ITEM 448.
- D. NO CONCRETE PAVEMENT WILL BE ACCEPTED

MINIMUM STANDARDS

ITEM	DESCRIPTION	ARTERIAL	COLLECTOR		MINOR
			COMM.& IND.	RESIDENTIAL	
I	RIGHT-OF-WAY	80'	60'	60'	50'
II	B\B CURB	59'	37'	37'	31'
III	SIDEWALK WIDTH	4'	4'	4'	4'
IV	CURB LAWN	8.0'	6.5'	6.5'	6.0'
V	ITEM 448	1-1/2"	1-1/2"	1-1/2"	1-1/2"
VI	ITEM 448	1-1/2"	1-1/2"	2"	2"
VII	ITEM 301	6"	3"	-	-
VIII	ITEM 304	2-4" LIFTS	2-4" LIFTS	2-6" LIFTS	2-5" LIFTS

CITY OF PIQUA

CHOICE ONE ENGINEERING

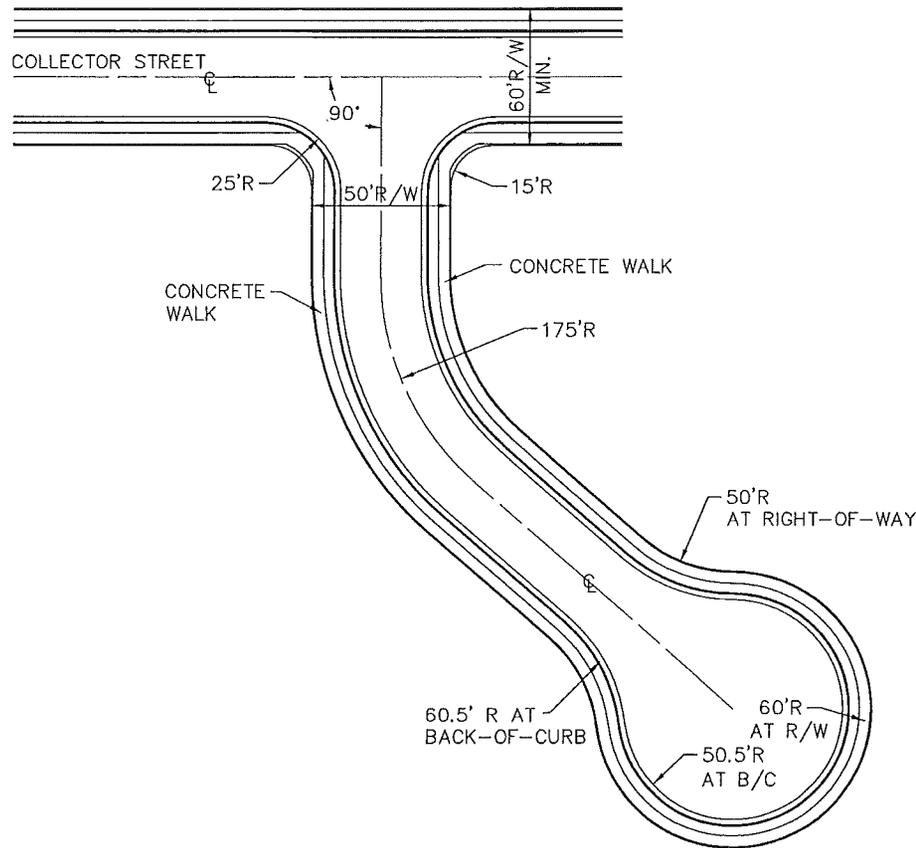
TYPICAL SECTIONS AND PAVEMENT COMPOSITION

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TYPICAL STREET AND CUL-DE-SAC PLAN

MINIMUM STREET DESIGN STANDARDS



	25 mph MINOR (THRU STREETS)	35 mph COLLECTOR	45 mph ARTERIAL
MINIMUM CENTERLINE GRADES	.50%	.50%	.50%
MAXIMUM CENTERLINE GRADES	10%	7%	4%
MINIMUM LENGTH OF VERTICAL CURVE (SEE NOTE A).	50FT.	50FT.	100FT.
MINIMUM CENTERLINE RADIUS	250FT.	400FT.	600FT.
MINIMUM LENGTH TANGENT BETWEEN CURVES	50FT.	50FT.	100FT.
MINIMUM BACK-OF-CURB RADIUS	25FT.	25FT.	50FT.
MINIMUM HORIZONTAL VISIBILITY	200FT.	300FT.	500FT.
MINIMUM STOPPING SIGHT DISTANCE (MEASURED FROM 3.5' EYE-LEVEL TO 6" OBJECT HEIGHT)	200FT.	300FT.	500FT.
CROSSROAD GRADE-STOP CONDITION- WITHIN 100' OF AN INTERSECTION	3%	3%	3%
RIGHT-OF-WAY WIDTH	50FT.	60FT.	80FT.

NOTES

A. MINIMUM LENGTH OF VERTICAL CURVE CAN BE REDUCED OR ELIMINATED TO ALLOW FOR PROPER DRAINAGE, WITH APPROVAL.

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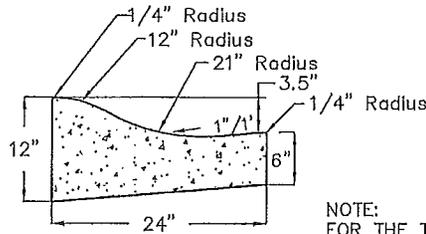
STREET DESIGN STANDARDS

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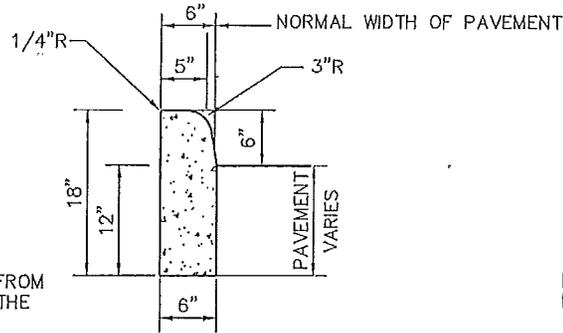
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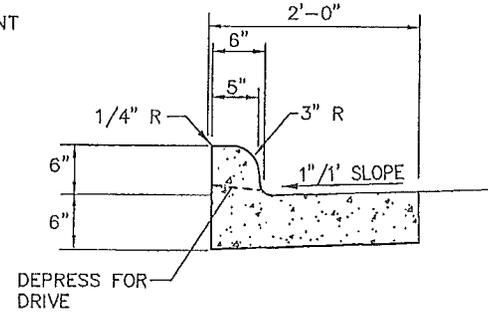


**CITY OF PIQUA
TYPE 1 CURB**

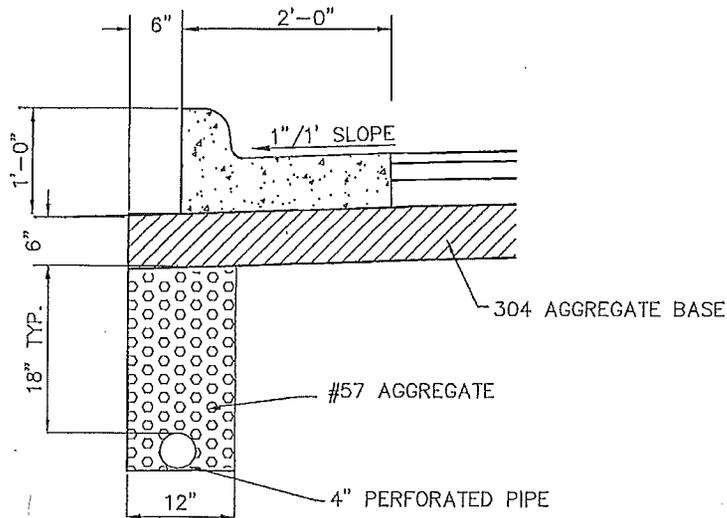
NOTE:
FOR THE TYPE 1 CURB,
THERE SHALL BE 3.5" FROM
THE TOP OF CURB TO THE
FACE OF GUTTER.



**TYPE 6
BARRIER CURB**



**TYPE 2 COMBINATION
CURB AND GUTTER**



**4" SHALLOW PIPE UNDERDRAIN DETAIL
(AS REQUIRED BY CITY)**

NOTES

- A. CONCRETE AND WORK SHALL MEET THE REQUIREMENT SET FORTH IN ODOT ITEM 609 CURBING.
- B. CURBING SHALL HAVE CONTRACTION JOINTS EVERY 10'.
- C. MINIMUM OF 6" OF ODOT 304 SHALL BE PLACED UNDER CURBING.
- D. CURBING SHALL BE BACKFILLED IMMEDIATELY AFTER FORMS ARE REMOVED OR AS SOON AS PRACTICAL WHEN SLIP FORMING PRIOR TO OTHER CONSTRUCTION OPERATIONS.
- E. PROVIDE BROOM FINISH AND EDGING TO ALL EXPOSED SURFACES.
- F. WITH PRIOR CITY DIRECTION, APPLY WHITE PIGMENTED CURING COMPOUND 80°F AIR TEMPERATURE OR CLEAR ON ALL SURFACES INCLUDING BACK IMMEDIATELY AFTER FINISHING SURFACES.
- G. CONCRETE SHALL BE ODOT CLASS C (4000 PSI, 600 LB/CY CEMENT). PROPORTIONING OPTIONS 1, 2, AND 3 NOT ALLOWED.
- H. CONCRETE SHALL CONTAIN 6% ± 1% OF TOTAL AIR.
- I. ALL CURBING PLACED SHALL HAVE A FRONT AND REAR FORM. EXCEPTION: UNLESS CURBING IS SLIPFORMED BY MACHINE OR IS ABUTTING CONCRETE DRIVEWAY OR SIDEWALK OR OTHERWISE APPROVED BY THE ENGINEER
- J. MINIMUM FLOW LINE SLOPE OF THE PERFORATED PIPE IS TO BE 0.003 FT/FT TO OUTLET

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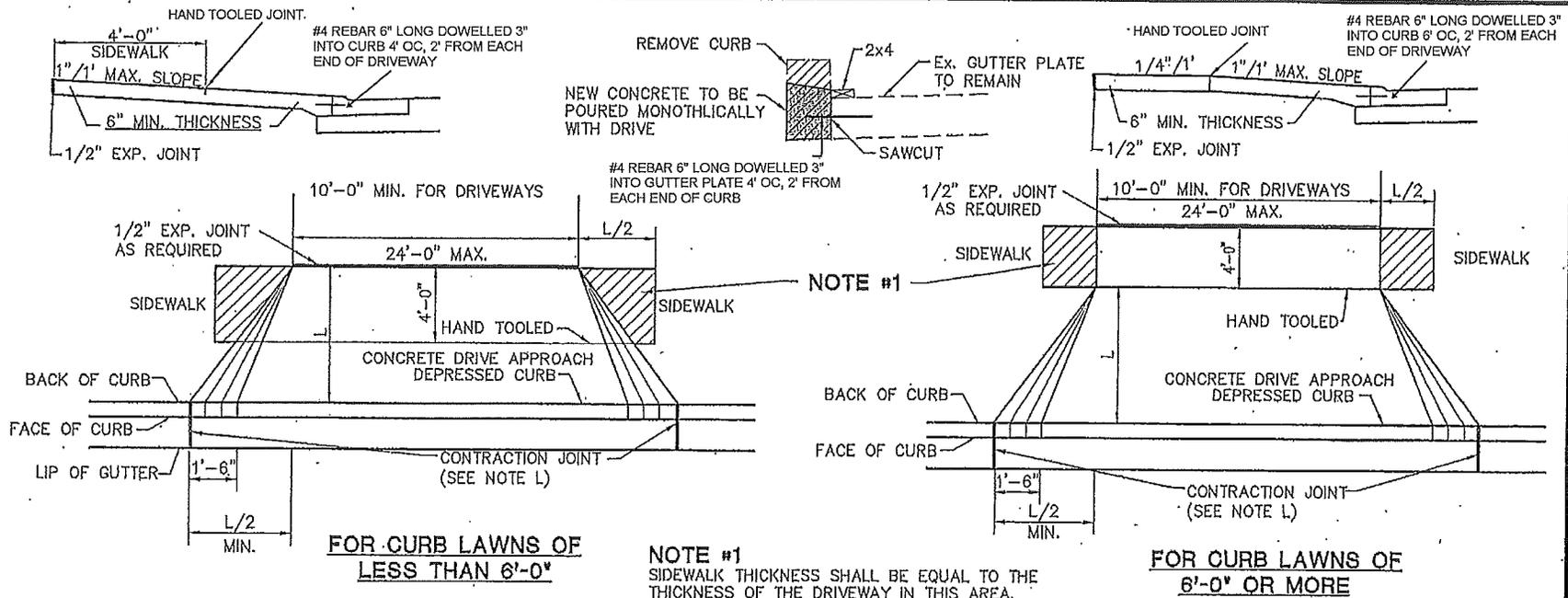
CONCRETE CURB DETAILS

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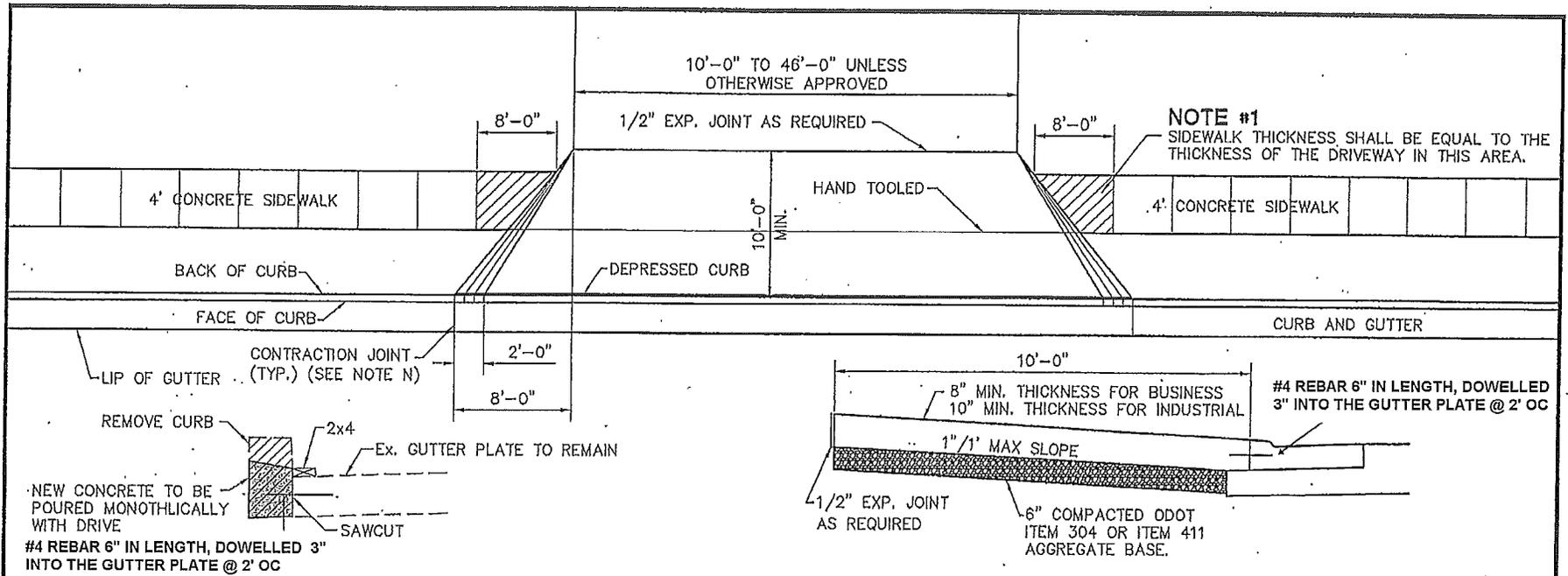
300-4



NOTES

- A. DRIVE APPROACHES SHALL MEET THE REQUIREMENTS OF ODOT ITEM 452, 499, 608, AND 609 CAST IN-PLACE CONCRETE.
- B. DRIVE APPROACHES MAY BE PLACED MONOLITHICALLY WITH CURB
- C. MAXIMUM JOINT SPACING SHALL BE 10' LONGITUDINALLY, TRANSVERSELY AND AT TAPERS.
- D. EXPANSION MATERIAL SHALL BE 1/2" PREMOLDED, ODOT APPROVED.
- E. 6" OF COMPACTED ODOT ITEM 304, ITEM 411 OR 57'S AGGREGATE BASE SHALL BE PLACED UNDER DRIVE APPROACHES.
- F. PROVIDE BROOM FINISH AND EDGING TO ALL EXPOSED SURFACES.
- G. WHERE CURB AND GUTTER HAS NOT BEEN DROPPED AT DRIVE APPROACHES, THE CONTRACTOR WILL CUT AND REMOVE CURB. (SEE DETAIL)
- H. WHERE ASPHALTIC CONCRETE PAVEMENT IS DISTURBED, THE ASPHALT SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE CITY.
- I. JOINTS SHALL BE CLEANED AND EDGED BY A 1/4" RADIUS EDGER. LONGITUDINAL JOINTS SHALL BE AS DIRECTED BY THE CITY. EXPANSION JOINTS SHALL BE OF SUCH DIMENSIONS AS SHOWN ON STANDARD DRAWINGS FOR CONSTRUCTION JOINTS.
- J. CONCRETE SHALL CONTAIN 6% ± 1% OF TOTAL AIR.
- K. CONCRETE SHALL BE ODOT CLASS C (4000 PSI, 600 LB/CY) CEMENT. PROPORTIONING OPTIONS 1, 2, & 3 NOT ALLOWED.
- L. THIS STANDARD DRAWING IS FOR GUIDELINE PURPOSES. EACH INDIVIDUAL DRIVE WILL NEED TO BE DESIGNED AND SUBMITTED TO THE CITY FOR REVIEW AND APPROVAL.
- M. "W"=10' MINIMUM TO 24' MAXIMUM UNLESS OTHERWISE APPROVED BY THE CITY.
- N. IF CURB IS REMOVED AND REPLACED DURING DRIVEWAY CONSTRUCTION, JOINTS BETWEEN EXISTING AND NEW CURB ARE TO BE DOWELLED WITH #4 REBAR 6" IN LENGTH, 3" INTO CURB, 4' OC
- O. ALL NEW CONSTRUCTION OR MODIFICATIONS OF DRIVE APPROACHES REQUIRE A CONCRETE APPROACH; REGARDLESS OF WHETHER THERE IS A SIDEWALK OR NOT. THE NEW APPROACH IS TO GO FROM EDGE OF EXISTING STREET TO RIGHT OF WAY OR A MINIMUM OF 6'-0".
- P. DRAINAGE ISSUES WILL HAVE TO BE ADDRESSED, WHEN A DRIVEWAY IS INSTALLED OR MODIFIED.
- Q. PRECAUTIONS SHALL BE TAKEN TO PROTECT EXISTING CONCRETE, BRICK, ETC. FROM TIRE MARKS AND DAMAGE DURING CONSTRUCTION.

CITY OF PIQUA CHOICE ONE ENGINEERING	<h2 style="margin: 0;">RESIDENTIAL DRIVE APPROACH</h2>	REVISIONS: 02/24/10 11/01/13	DATE APPROVED: AUG, 2008 PAGE No. 300-5
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NOTE #1
SIDEWALK THICKNESS SHALL BE EQUAL TO THE THICKNESS OF THE DRIVEWAY IN THIS AREA.

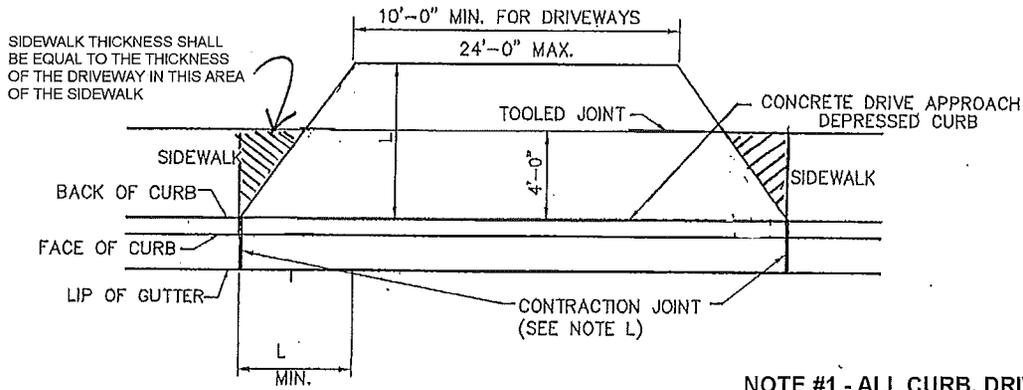
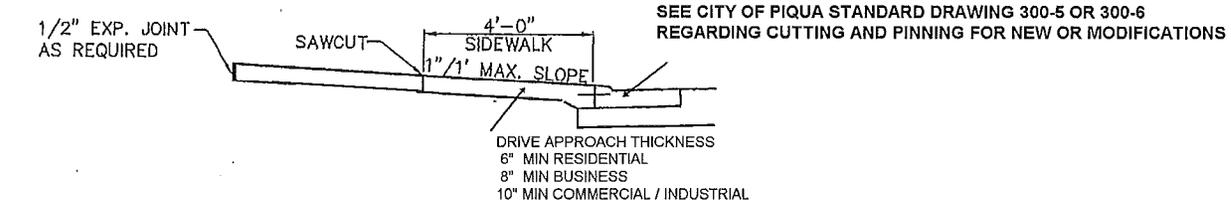
NOTES

- A. DRIVE APPROACHES SHALL MEET THE REQUIREMENTS OF ODOT ITEM 452 AND 499 CAST IN PLACE CONCRETE.
- B. DRIVE APPROACHES MAY BE PLACED MONOLITHICLY WITH CURB.
- C. MAXIMUM JOINT SPACING SHALL BE 10' LONGITUDINALLY AND TRANSVERSELY WITH JOINTS AT TAPERS.
- D. EXPANSION MATERIAL SHALL BE 1/2" PREMOLDED.
- E. 6" OF COMPACTED ODOT ITEM 304 OR ITEM 411 AGGREGATE BASE SHALL BE PLACED UNDER DRIVE APPROACHES.
- F. PROVIDE BROOM FINISH AND EDGING TO ALL EXPOSED SURFACES.
- G. WHERE CURB AND GUTTER HAS NOT BEEN DROPPED AT DRIVE APPROACHES, THE CONTRACTOR WILL CUT AND REMOVE CURB (SEE DETAIL).
- H. WHERE ASPHALTIC CONCRETE PAVEMENT IS DISTURBED, THE ASPHALT SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE CITY.
- I. JOINTS SHALL BE CLEANED AND EDGED BY A 1/4" RADIUS EDGER. LONGITUDINAL JOINTS SHALL BE AS DIRECTED BY THE CITY. EXPANSION JOINTS SHALL BE OF SUCH DIMENSIONS AS SHOWN ON STANDARD DRAWINGS FOR CONSTRUCTION JOINTS.
- J. MINIMUM WIDTH FOR ONE-WAY TRAFFIC IS 16'-0". MINIMUM WIDTH FOR TWO-WAY TRAFFIC IS 25'-0". MAXIMUM WIDTH IS 36'-0" FOR BUSINESS, AND 46'-0" FOR INDUSTRIAL UNLESS OTHERWISE APPROVED BY THE CITY.
- K. THIS STANDARD DRAWING IS FOR GUIDELINE PURPOSES. EACH INDIVIDUAL DRIVE WILL NEED TO BE DESIGNED AND SUBMITTED TO THE CITY FOR REVIEW AND APPROVAL.
- L. CONCRETE SHALL BE ODOT CLASS C. (4000 PSI, 800 LB/CY CEMENT, PROPORTIONING OPTIONS 1, 2, & 3 NOT ALLOWED.
- M. CONCRETE SHALL CONTAIN 6% ± 1% OF THE TOTAL AIR.
- N. IF CURB IS REMOVED AND REPLACED DURING DRIVEWAY CONSTRUCTION, JOINTS BETWEEN CURB AND DRIVEWAY ARE TO BE DOWELLED WITH #4 REBAR 6" IN LENGTH, DOWELLED 3" INTO THE GUTTER PLATE @ 2' OC
- O. ALL NEW CONSTRUCTION OR MODIFICATIONS OF DRIVE APPROACHES REQUIRE A CONCRETE APPROACH, REGARDLESS OF WHETHER THERE IS A SIDEWALK OR NOT. THE NEW APPROACH IS TO GO FROM EDGE OF EXISTING STREET TO RIGHT OF WAY OR A MINIMUM OF 10'-0".
- P. DRAINAGE ISSUES WILL HAVE TO BE ADDRESSED, WHEN A DRIVEWAY IS INSTALLED OR MODIFIED.
- Q. PRECAUTIONS SHALL BE TAKEN TO PROTECT EXISTING CONCRETE, BRICK, ETC. FROM TIRE MARKS AND DAMAGE DURING CONSTRUCTION.

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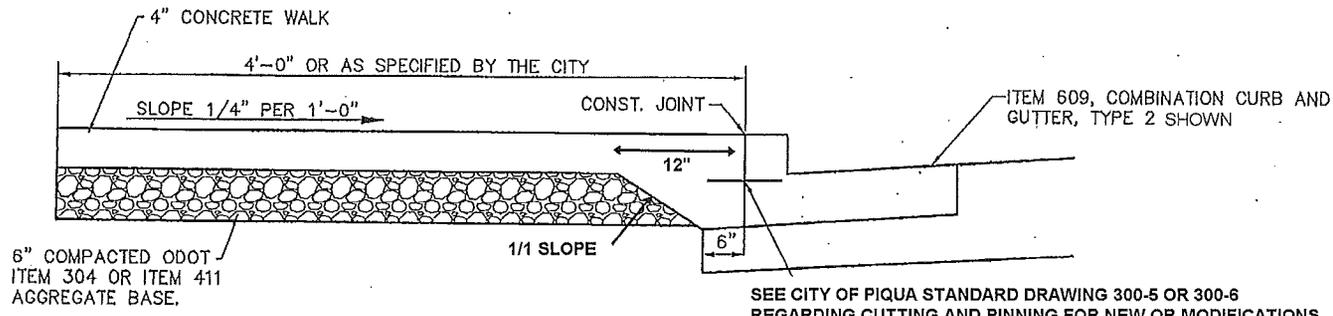
**COMMERCIAL AND INDUSTRIAL
DRIVE APPROACH**

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DRIVE APRON WITH NO CURB LAWN
FOR DRIVEWAY SPECIFICATIONS
SEE PAGE 300-5 OR 300-6

NOTE #1 - ALL CURB, DRIVEWAY OR SIDEWALK CONSTRUCTION SHALL COMPLY WITH CITY OF PIQUA STANDARD DRAWINGS 300-04, 300-05, AND 300-06



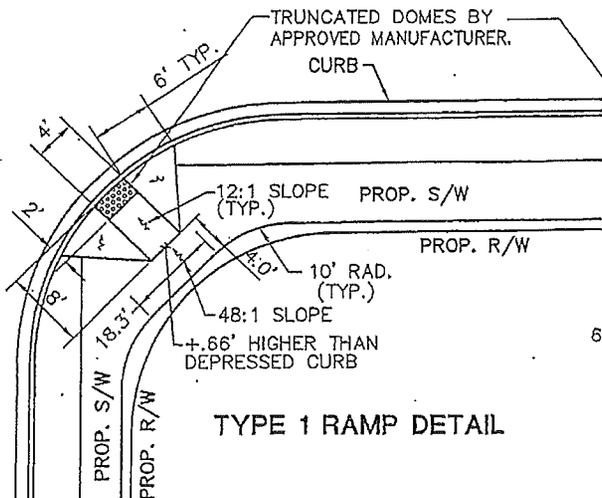
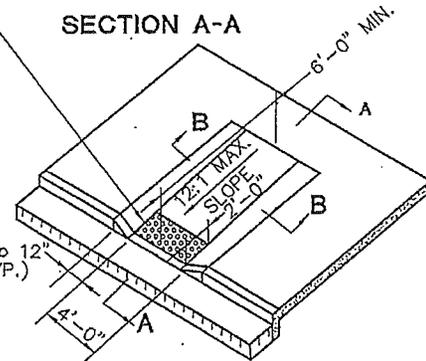
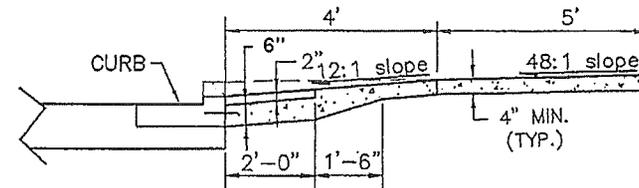
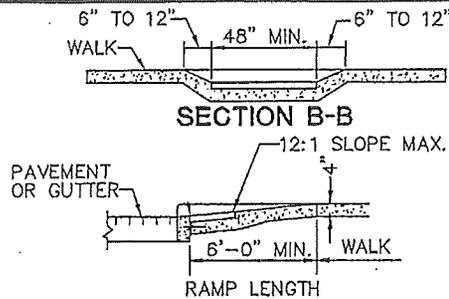
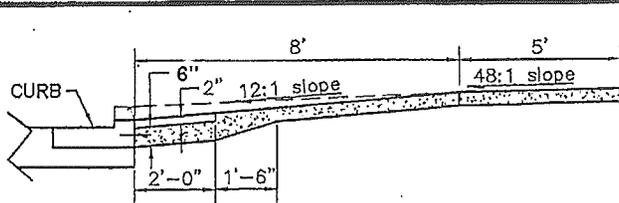
SEE CITY OF PIQUA STANDARD DRAWING 300-5 OR 300-6 REGARDING CUTTING AND PINNING FOR NEW OR MODIFICATIONS

CONCRETE SIDEWALK ABUTTING CURB, COP Type #2 curb shown

CITY OF PIQUA
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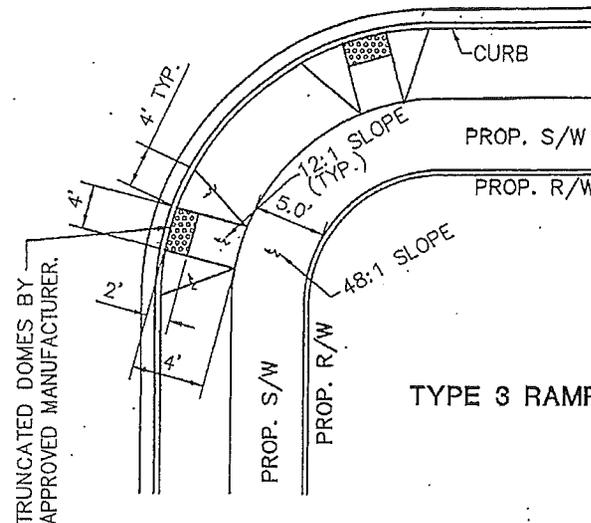
DRIVE APPROACH AND CONCRETE SIDEWALK DETAIL WITH NO CURB LAWN

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TYPE 1 RAMP DETAIL

TYPE 2 RAMP DETAIL



TYPE 3 RAMP DETAIL

NOTES

- A. CITY TO SPECIFY TYPE 1, 2, OR 3 CURB RAMP.
- B. ANY COMBINATION OF SIDE SLOPES ON OPPOSITE SIDES OF A RAMP MAY BE USED TO BEST FIT THE SITE CONDITIONS.
- C. THE MINIMUM RAMP LENGTH IS 6' FROM BACK OF A 6" CURB AND MAY BE INCREASED WHERE FEASIBLE TO OBTAIN A FLATTER RAMP SLOPE OR TO BETTER BLEND WITH THE WALK CONFIGURATION.
- D. WALK THICKNESS IN THE RAMP SLOPES SHALL BE 6" MINIMUM OR THICKER AS NECESSARY TO MATCH ADJACENT WALK THICKNESS.
- E. CURB RAMPS SHALL MEET AND BE FINISHED TO AMERICANS WITH DISABILITIES ACT (A.D.A.) STANDARDS.
- F. TEXTURE OF CONCRETE SURFACE SHALL BE OBTAINED BY COURSE BROOMING TRAVERSE TO THE RAMP SLOPES AND SHALL BE ROUGHER THAN ADJACENT WALK.

- G. CURB RAMPS SHALL MEET THE REQUIREMENTS OF ODOT ITEM 608 UNLESS OTHERWISE SPECIFIED WITHIN. REFER TO ODOT STANDARD DRAWINGS FOR ADDITIONAL CURB RAMP CONFIGURATIONS.
- H. CONCRETE SHALL BE ODOT CLASS C (4000 PSI, 600 LB/CY CEMENT), PROPORTIONING OPTIONS 1, 2, AND 3 NOT ALLOWED.
- I. CONCRETE SHALL CONTAIN 6% ± 1% OF TOTAL AIR.
- J. FOR RECONSTRUCTION JOBS, THE CURB RAMPS WILL HAVE TO BE ADDRESSED BASED ON THE EXISTING CONDITIONS.
- K. TRUNCATED-DOME SPECIFICATIONS:
INSTALL DETECTABLE WARNINGS (TRUNCATED DOMES) FOR A DISTANCE OF 24" FROM THE BACK OF CURB FOR THE ENTIRE WIDTH OF THE RAMP OPENING WHERE IT IS FLUSH WITH THE PAVEMENT. THE TRUNCATED DOMES SHALL BE ODOT APPROVED, THE COLOR OF THE PANEL SHALL BE RED.

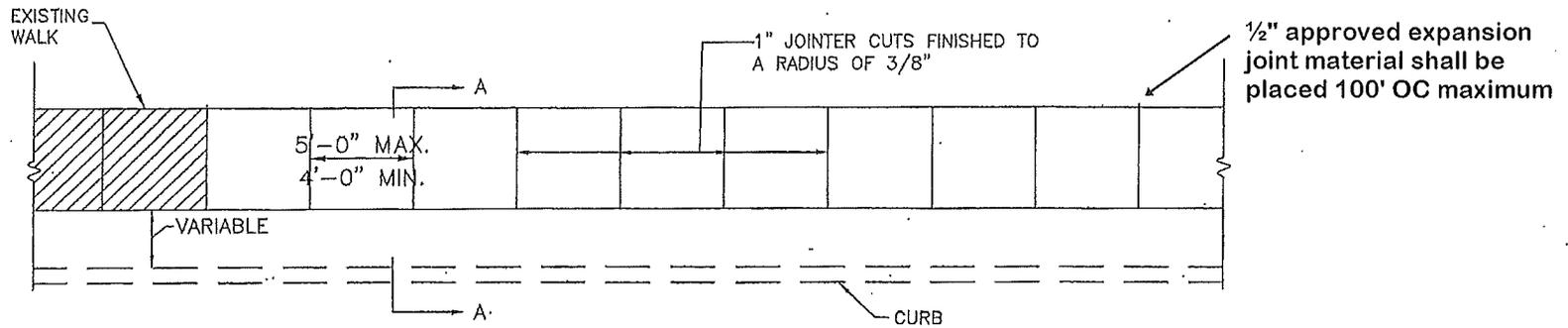
- L. CURB RAMPS TO BE POURED ON COMPACTED UNDISTURBED EARTH OR IF OVER EXCAVATED BY THE CONTRACTOR THE CURB RAMP SHALL BE POURED ON COMPACTED GRANULAR BEDDING. CITY INSPECTION OF FORM WORK IS REQUIRED PRIOR TO PLACING CONCRETE.
- M. PROVIDE BROOM FINISH AND EDGING TO ALL EXPOSED SURFACES. TEXTURE SHALL BE A MEDIUM BROOM WITH TOOL FINISH, CITY TO REVIEW AND APPROVE FIRST POUR.
- N. ALL DETECTABLE WARNING CURB RAMPS TO BE CAST IRON MANUFACTURED BY THE EAST JORDAN IRON WORKS 7005 SERIES, NEENAH FOUNDRY MODEL R-4984 OR A PREAPPROVED EQUAL.
- O. ALL RAMPS SHALL BE DOWELLED TO THE CURB BY THE USE OF 6" #4 REBAR AS PER CITY OF PIQUA STANDARD 300-07. CONCRETE SIDEWALK ABUTTING TYPE 2 CURB DETAIL.

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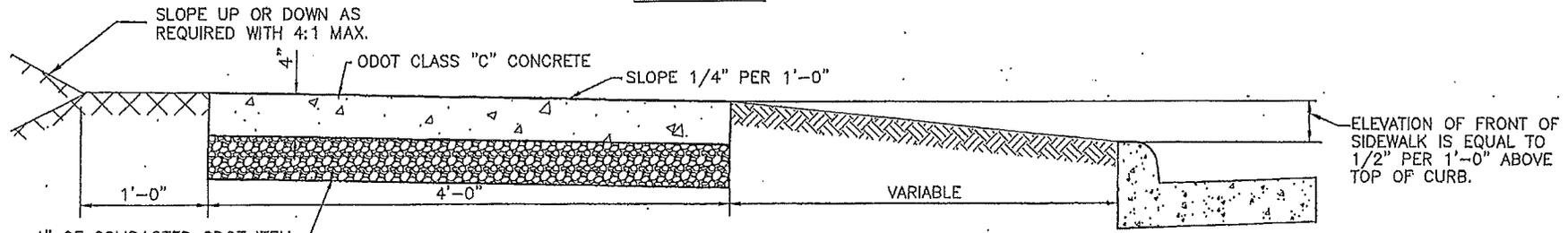
CURB RAMPS

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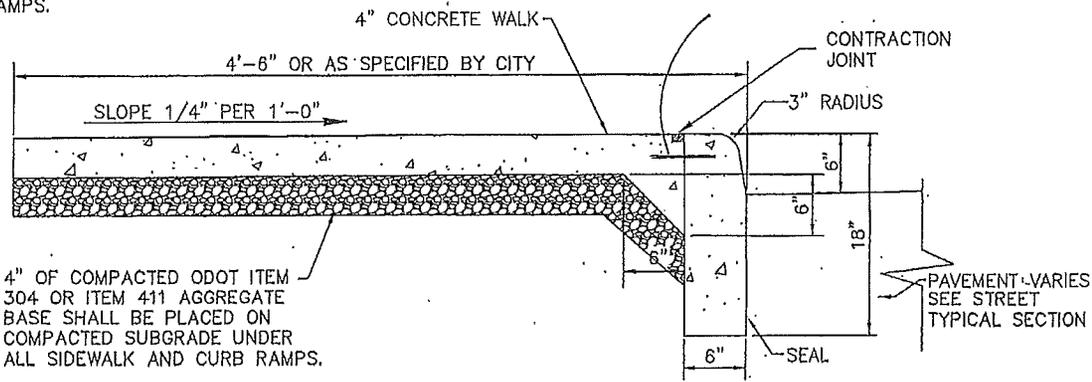
PLAN VIEW



SECTION A-A

4" OF COMPACTED ODOT ITEM 304 OR ITEM 411 AGGREGATE BASE SHALL BE PLACED ON COMPACTED SUBGRADE UNDER ALL SIDEWALK AND CURB RAMPS.

6" - #4 Rebar dowelled 3" into curbing 6' OC



COMBINED CURB AND SIDEWALK DETAIL

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CONCRETE SIDEWALK DETAIL

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NOTES

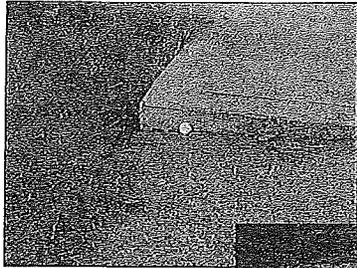
- A. WALK TO BE POURED ON UNDISTURBED EARTH OR COMPACTED GRANULAR BEDDING.
- B. PROVIDE BROOM FINISH TO ALL EXPOSED SURFACES.
- C. CONCRETE SHALL CONFORM TO ODOT ITEM 499 CONCRETE. CONCRETE WORK SHALL CONFORM TO ODOT ITEM 608, UNLESS OTHERWISE SPECIFIED WITHIN.
- D. PROVIDE EDGING AROUND ALL EXPOSED SURFACES.
- E. APPLY ONE COAT OF A CLEAR ODOT APPROVED CONCRETE SEALER ON ALL SURFACES INCLUDING THE BACK PER THE MANUFACTURES RECOMMENDATIONS.
- F. WHEN RENOVATING EXISTING STREETS, THE SIDEWALKS SHALL BE REPLACED TO CONFORM WITH CITY CONSTRUCTION STANDARDS AND DRAWINGS.
- G. CONCRETE SHALL BE ODOT CLASS C (4000 PSI, 600 LB/CY CEMENT) PROPORTIONING OPTIONS 1, 2, & 3 NOT ALLOWED.
- H. CONCRETE SHALL CONTAIN 6% ± 1% OF TOTAL AIR.
- I. PROPERTY PINS SHALL BE REESTABLISHED AFTER FINISHING OF SIDEWALK.

SIDEWALK JOINTS

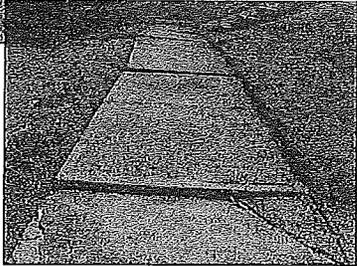
- 1. GENERAL: CONSTRUCT ISOLATION, CONSTRUCTION, AND CONTRACTION JOINTS, AND TOOL EDGINGS TRUE TO LINE WITH FACES PERPENDICULAR TO SURFACE PLANE OF CONCRETE. CONSTRUCT TRANSVERSE JOINTS AT RIGHT ANGLES TO CENTERLINE, UNLESS OTHERWISE INDICATED.
 - A. WHEN JOINING EXISTING PAVEMENT, PLACE TRANSVERSE JOINTS TO ALIGN WITH PREVIOUSLY PLACED JOINTS, UNLESS OTHERWISE INDICATED.
- 2. CONSTRUCTION JOINTS: SET CONSTRUCTION JOINTS AT SIDE AND END TERMINATION OF PAVEMENT AND AT LOCATIONS WHERE PAVEMENT OPERATIONS ARE STOPPED FOR MORE THAN ONE-HALF HOUR, UNLESS PAVEMENT TERMINATES AT ISOLATION JOINTS.
- 3. EXPANSION JOINTS: FORM ISOLATION JOINTS OF PREFORMED JOINT-FILLER STRIPS ABUTTING MANHOLES, STRUCTURES, WALKS, OTHER FIXED OBJECTS, AND WHERE INDICATED. EXPANSION JOINTS SHALL NOT BE PLACED AT THE BUILDING FACE UNLESS DIRECTED BY THE CITY OF PIQUA.
 - A. LOCATION OF EXPANSION JOINTS AT INTERVALS OF 60', UNLESS OTHERWISE INDICATED.
 - B. LOCATE EXPANSION JOINTS ALONG BUILDING.
 - C. THE EXPANSION JOINT MATERIAL SHALL BE 1/2" THICK PER ODOT 705.33.
- 4. CONTRACTION JOINTS: FORM WEAKENED-PLANE CONTRACTION JOINTS, SECTIONING CONCRETE INTO AREAS AS INDICATED IN THE PLANS. CONSTRUCT CONTRACTION JOINTS FOR A DEPTH EQUAL TO AT LEAST ONE-FOURTH OF THE CONCRETE THICKNESS, WHERE INDICATED, AS FOLLOWS:
 - A. GROOVED JOINTS: FORM CONTRACTION JOINTS AFTER INITIAL FLOATING BY GROOVING AND FINISHING EACH EDGE OF JOINT WITH GROOVER TOOL TO THE FOLLOWING RADIUS. REPEAT GROOVING OF CONTRACTION JOINTS AFTER APPLYING SURFACE FINISHES. ELIMINATE GROOVER OVERFLOW SLURRY MARKS ON CONCRETE SURFACES. QUALITY WORK SHALL BE PERFORMED OR THE NEW SIDEWALK WILL BE REMOVED AND REDONE AT THE CONTRACTOR'S EXPENSE. RADIUS TO BE 1/4 INCH (6 MM).
 - B. SAWED JOINTS WILL NOT BE PERMITTED.
- 5. EDGING: TOOL EDGES OF JOINTS IN CONCRETE AFTER INITIAL FLOATING WITH AN EDGING TOOL TO A RADIUS OF 1/4 INCH (6 MM). REPEAT TOOLING OF EDGES AFTER APPLYING SURFACE FINISHES. ELIMINATE TOOL MARKS (OVERFLOW SLURRY) ON CONCRETE SURFACES.

CITY OF PIQUA	<h2>CONCRETE SIDEWALK NOTES</h2>	REVISIONS: 04/02/14	DATE APPROVED: AUG. 2008
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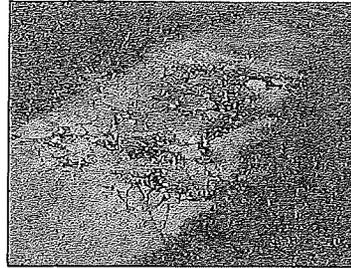
EXAMPLE: 1/2 INCH TRIP HAZARD



ADJOINING BLOCKS OR PORTIONS THEREOF WHOSE EDGES DIFFER VERTICALLY BY MORE THAN 1/2 INCH.



EXAMPLE: DETERIORATION

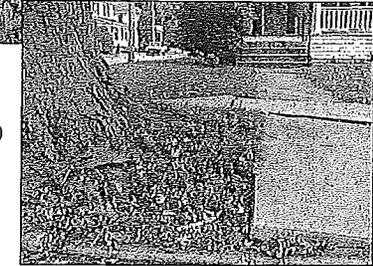


ANY SIDEWALK THAT IS DETERIORATED OR SHOWS SURFACE SPALLING, LEAVING IT VERY ROUGH, UNSAFE, OR WITH AGGREGATE PROTRUDING.

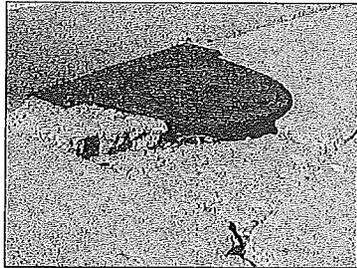
EXAMPLE: ABRUPT SLOPE



BLOCKS, OR PORTION OF BLOCKS, THAT CAUSE AN ABRUPT CHANGE OF 1 INCH PER FOOT (OR MORE) IN ANY DIRECTION OF THE SIDEWALK.

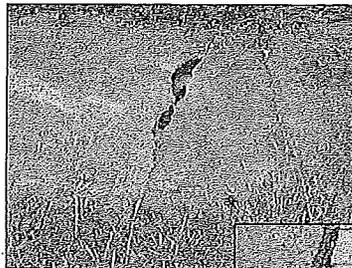


EXAMPLE: PLATES, COVERS, ETC.

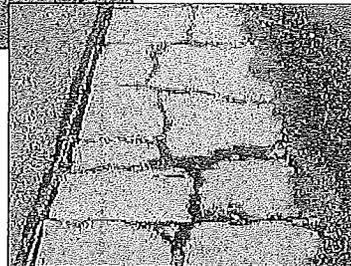


METAL OR OTHER PLATES, COVERS, OR GRATINGS THAT ARE NOT FLUSH (3/4 INCH OR MORE VERTICAL DIFFERENCE) WITH THE ADJOINING SIDEWALK SURFACE, ARE STRUCTURALLY UNSAFE, OR CAUSE A NUISANCE DUE TO SLIPPERY SURFACES ETC.

EXAMPLE: CRACKS



ANY SIDEWALK BLOCK (BASED ON 20 SQ. FT.) HAVING A CRACK OR CRACKS IN IT OF AT LEAST 3/4 INCH WIDE WITH A MINIMUM OF 4 LINEAL FEET IN ONE BLOCK. (VARIOUS SIZE BLOCKS WILL BE EVALUATED PROPORTIONALLY.)



PERMITS, INSPECTION, AND WORK RULES

- A. NO PERSON SHALL TEAR UP OR DIG INTO ANY PUBLIC RIGHT-OF-WAY OR STREET FOR THE PURPOSE OF CONSTRUCTING OR REPAIRING THE SIDEWALK, CURBING, OR GUTTERS THEREON OR FOR ANY OTHER PURPOSE, WITHOUT HAVING FIRST OBTAINED A PERMIT FROM THE ENGINEERING DEPARTMENT TO DO SO.
- B. THE CONTRACTOR MUST CALL THE CITY FOR AN INSPECTION AT LEAST FOUR WORKING HOURS BEFORE HE PLANS TO POUR THE CONCRETE. THE CONTRACTOR OR HIS FOREMAN MUST BE ON THE JOB WHEN THE INSPECTOR ARRIVES. IF, BECAUSE OF WEATHER CONDITIONS OR FOR SOME OTHER REASON, IT WILL NOT BE POSSIBLE TO HAVE A PERSON ON THE JOB, THE CONTRACTOR IS REQUIRED TO CALL AND CANCEL THE INSPECTION.
- C. THE CONTRACTOR IS CAUTIONED AGAINST ORDERING CONCRETE BEFORE THE INSPECTION IS MADE DUE TO POSSIBLE CORRECTION OF FORMS OR GRADE.
- D. THE CONTRACTOR SHALL PROVIDE PROTECTION AND TRAFFIC CONTROL BARRICADES, LIGHTS, SIGNS, AND OTHER DEVICES AS HEREIN SPECIFIED TO PROVIDE WARNING AND PROTECTION FOR VEHICULAR TRAFFIC, PEDESTRIANS, AND THE WORK DURING THE REMOVAL, CONSTRUCTION AND CURING OF SIDEWALK, CURB AND GUTTER, AND DRIVEWAY APRONS.
- E. THE CONTRACTOR WILL BE RESPONSIBLE FOR AN IMMEDIATE REMOVAL AND CLEANUP OF ALL EXCAVATED MATERIAL. NO EXCAVATED MATERIAL SHALL BE STORED ON THE PAVEMENT.
- F. ALL CONTRACTORS INSTALLING NEW CURB ARE CAUTIONED THAT IT IS THEIR RESPONSIBILITY TO REPAIR THE STREET PER CITY SPECIFICATIONS BEFORE REMOVING YOUR BARRICADES.

CITY OF PIQUA

CHOICE ONE ENGINEERING

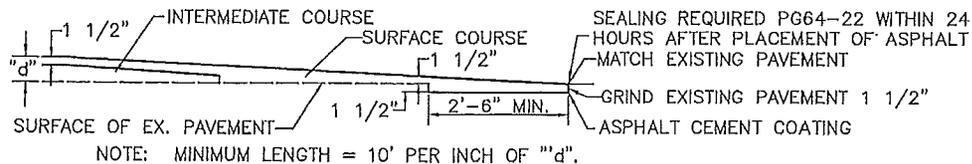
GUIDELINES FOR REPLACEMENT OF SIDEWALKS

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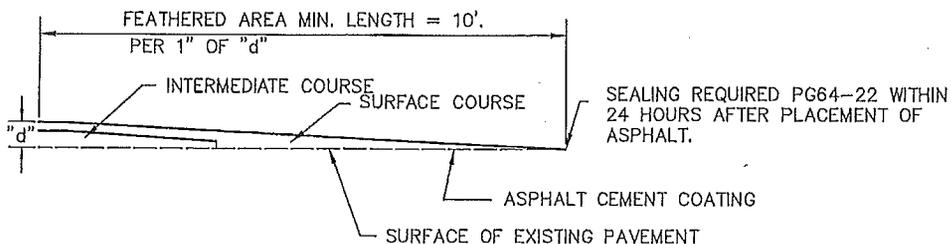
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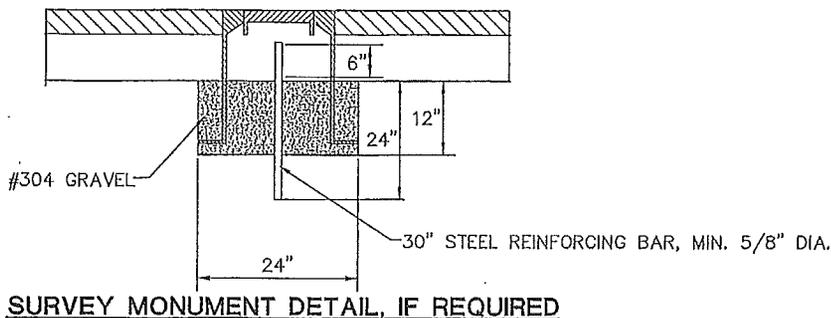
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BUTT JOINT DETAIL



FEATHERING DETAIL

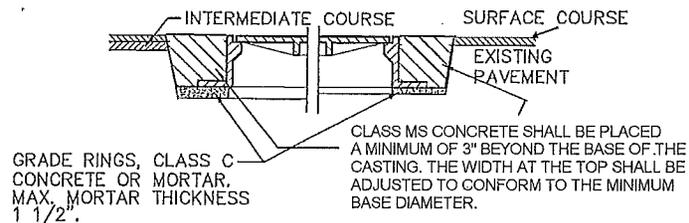


SURVEY MONUMENT DETAIL, IF REQUIRED

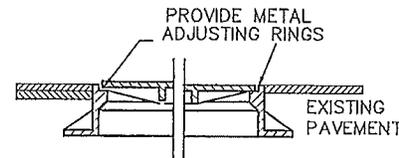
NOTES

- A. IF REQUIRED, MONUMENT BOXES SHALL BE SET PRIOR TO THE LAYING OF ODOT ITEM 448 ASPHALT UNLESS OTHERWISE PREAPPROVED.
- B. MONUMENT ASSEMBLIES SHALL BE NEENAH R-1978-A2 OR EAST JORDAN 8375.
- C. MONUMENT BOXES SHALL MEET THE REQUIREMENTS OF ODOT ITEM 604 UNLESS OTHERWISE SPECIFIED WITHIN.

MANHOLES ADJUSTED TO GRADE FOR OVERLAYS



USING CONCRETE OR MORTAR



USING METAL ADJUSTING RINGS

NOTES

- METAL ADJUSTING RINGS SHALL:
 - A. ATTACH SECURELY TO THE EXISTING FRAME BY TACK WELD OR MECHANICAL DEVICES.
 - B. CONSIST EITHER OF CAST METAL HAVING AN INTEGRAL RIM AND SEAT, OR BE FABRICATED METAL WITH A STURDY CONNECTION BETWEEN THE SEAT AND RIM.
 - C. PROVIDE AN EVEN SEAT FOR THE MANHOLE COVER
 - D. SHALL BE A SOLID ONE PIECE RING ACCEPTABLE TO THE CITY OF PIQUA. ADJUSTABLE DIAMETER RINGS WILL NOT BE ACCEPTABLE
 - E. ANY INSTALLATION UNACCEPTABLE TO THE CITY SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.

CITY OF PIQUA	ASPHALT OVERLAY AND MONUMENT	REVISIONS: 11/01/13	DATE APPROVED: AUG. 2008
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GENERAL

A. ALL STREET CONSTRUCTION SHALL BE IN ACCORDANCE WITH ODOT SPECIFICATIONS, LATEST REVISION.

B. CONTRACTOR MUST APPLY FOR NECESSARY PERMITS, FEES, ETC. WITH THE CITY BEFORE CONSTRUCTION OR DEMOLITION BEGINS.

PAVEMENT REPLACEMENT

A. IMMEDIATELY AFTER PLACEMENT OF BACKFILL IN EXISTING STREETS, A TEMPORARY PAVEMENT SHALL BE INSTALLED AND THE STREET OPENED. TEMPORARY PAVEMENT SHALL CONSIST OF 8" OF COMPACTED ODOT SPECIFICATION 411 BASE AND A SURFACE COURSE APPROVED BY THE CITY. THE SURFACE SHALL BE KEPT FLUSH WITH THE EXISTING STREET.

B. PERMANENT PAVEMENT REPLACEMENT SHALL BE EQUAL TO OR EXCEED THE EXISTING PAVEMENT. (MINIMUM PAVEMENT COMPOSITION, SEE PAGE 300-2).

C. ANY SETTLEMENT OF A TRENCH CAUSING A DEPRESSION SHALL BE REFILLED AS REQUIRED BY THE CITY AT THE CONTRACTOR'S EXPENSE. THIS PROVISION APPLIES FOR A ONE-YEAR PERIOD AFTER WORK IS ACCEPTED BY THE CITY.

D. ALL TEMPORARY PAVEMENT AND SIDEWALK SHALL BE MAINTAINED BY THE CONTRACTOR OR DEVELOPER AT HIS OWN EXPENSE IN A SUITABLE AND SAFE CONDITION FOR TRAFFIC UNTIL PERMANENT REPLACEMENT IS MADE OR THE PROJECT IS FINALLY ACCEPTED BY THE CITY. COLD PATCH ALL TRENCHES A MINIMUM OF 3" WHEN FINAL ASPHALT WILL NOT BE REPLACED WITHIN 24 HOURS.

TRAFFIC CONTROL

A. THE CONTRACTOR SHALL MAINTAIN TRAFFIC CONTROL AT ALL TIMES WITH THE PROPER BARRICADES AS PER THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THESE CONTROL DEVICES SHALL BE IN PLACE PRIOR TO ANY WORK COMMENCING.

B. TRAFFIC SHALL BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE APPROVED BY THE CITY.

CURB STAKING AND ROADWAY

A. LINE AND GRADE EVERY 25' ON A CONVENIENT OFFSET.

PAVEMENT (ASPHALT)

A. THE CONTRACTOR SHALL PROVIDE THE CITY WITH A COPY OF THE NORMAL (MEDIUM TRAFFIC) ODOT 448 JOB MIX FORMULA FOR EACH PLANT THAT PROVIDES HOT MIXED ASPHALT TO THIS PROJECT. ALL MIXES SHALL FOLLOW ODOT JOB MIX FORMULA.

B. ALL WORK SHALL ADHERE TO ODOT'S LATEST REVISIONS AND TO THE CITY SPECIFICATIONS WHICHEVER IS MORE STRINGENT SHALL PREVAIL UNLESS OTHERWISE APPROVED.

C. PATCHED AREAS SHALL BE SEALED ON THE PERIMETER OF THE PATCH WITH ASPHALT CEMENT.

D. ALL UTILITY ADJUSTMENTS -- MANHOLE, WATER VALVE, ETC., -- SHALL BE RAISED TO FINISHED GRADE AFTER THE FINAL ASPHALT COURSE IS LAID. IF SMOOTHNESS DOESN'T MEET CITY'S REQUIREMENTS A REPAIR SIMILAR TO MR. MANHOLE SHALL BE USED TO ADJUST MANHOLE OR VALVE.

E. ASPHALT CEMENT SHALL BE USED NEXT TO THE LIP OF GUTTER PRIOR TO THE FINAL ASPHALT LIFT BEING PLACED. (SS-1 TACK OR PG64-22 SEAL.)

F. TACK COAT SHALL BE APPLIED PRIOR TO THE PLACEMENT OF THE FINAL LIFT OF ASPHALT IF THE EXISTING ASPHALT LIFT IS DIRTY OR AFTER TEN DAYS UNLESS OTHERWISE APPROVED. TEMPERATURE MUST BE 50°F OR HIGHER.

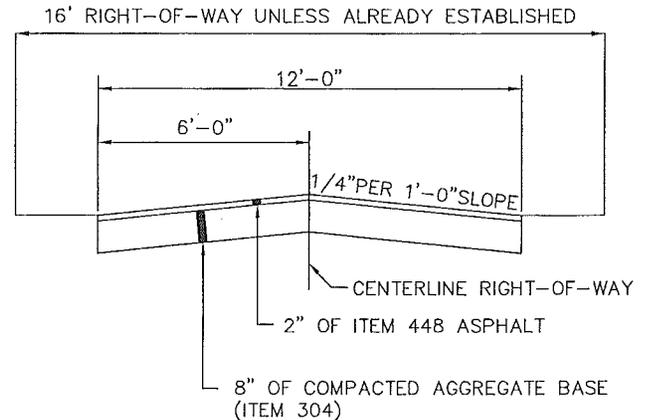
G. PRIME COAT SHALL BE APPLIED ON NEW AGGREGATE WHEN TEMPERATURE IS 50°F OR HIGHER. ALL DRIVEWAYS SHALL BE PRIMED PRIOR TO PLACEMENT OF ASPHALT UNLESS OTHERWISE APPROVED.

H. NO ASPHALT SHALL BE PLACED OVER EXCAVATED TRENCHES UNLESS TRENCHES HAVE BEEN COMPACTED AS PER CITY SPECIFICATIONS.

I. FINAL LIFT OF ASPHALT SHALL BE FINISHED TO 1/4" ABOVE THE LIP OF GUTTER.

J. ASPHALT CEMENT SHALL BE USED ON ALL JOINTS AND FEATHERED SURFACES PRIOR TO PLACEMENT OF THE NEXT COURSE OF ASPHALT TO THE ABUTTING JOINT, UNLESS OTHERWISE APPROVED.

K. ALL EDGES TO BE TRIMMED BACK TO SOLID MATERIAL BY SAWING AND BE STRAIGHT AND NEAT AS PER THE CITY'S INSTRUCTIONS.



TYPICAL ALLEY CONSTRUCTION

- A.** MINIMUM STANDARD (UNLESS OTHERWISE APPROVED.)
- B.** FOR RENOVATION OF EXISTING ALLEYS ONLY. NO NEW ALLEYS WILL BE APPROVED WITHIN THE CITY.

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**MISCELLANEOUS ROADWAY NOTES
AND ALLEY DETAIL**

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Permit Required

A RIGHT-OF-WAY PERMIT FOR ANY DIGGING OR EXCAVATION WITHIN A PUBLIC RIGHT-OF-WAY FOR ANY STREET OR ALLEY IS REQUIRED 48 HOURS IN ADVANCE OF THE WORK. IN THE EVENT OF AN EMERGENCY, THE PERMIT APPROVAL REQUIREMENT SHALL BE WAIVED AND THE PROPER APPLICATION MUST BE SUBMITTED AS SOON AS POSSIBLE, BUT NO LATER THAN THE END OF THE FIRST WORKING WEEKDAY AFTER THE START OF WORK. AN EMERGENCY IS DEFINED AS A REPAIR REQUIRED TO PROVIDE SERVICE TO UTILITY CUSTOMERS OR TO MITIGATE A HAZARD, WHICH THREATENS PUBLIC HEALTH OR SAFETY.

PERMIT FORMS ARE AVAILABLE FROM THE CITY STREET DEPARTMENT. THE PERMIT FORM IS TO BE COMPLETED BY THE PERSON OR FIRM PLANNING THE WORK WITHIN THE RIGHT-OF-WAY. ALL FEES MUST BE PAID AND APPROVALS OBTAINED BEFORE ANY WORK IS STARTED. A 72 WORKING HOUR LEAD-TIME IS RECOMMENDED. A PERMIT FEE OF \$25 WILL BE REQUIRED OF EACH APPLICANT, INCLUDING CITY DEPARTMENTS.

PERFORMANCE BOND

ANY INDIVIDUAL OR FIRM WHO MAKES APPLICATION FOR A RIGHT-OF-WAY OPENING PERMIT MUST PROVIDE A CURRENT PERFORMANCE BOND TO THE CITY OF PIQUA IN THE AMOUNT OF \$3,000.

NO BOND IS REQUIRED FOR PLUMBERS WHO HOLD A CURRENT PLUMBING LICENSE IN THE CITY OF PIQUA AND HAVE A CURRENT PERFORMANCE BOND ON FILE IN THE HEALTH DEPARTMENT.

IN THE EVENT THAT AFTER NOTIFICATION FROM THE CITY ANY CONTRACTOR FAILS TO CORRECT PROBLEMS ASSOCIATED WITH POOR TRENCH REPAIR OR MAINTENANCE WITHIN 24 HOURS OF NOTIFICATION, THE CITY RESERVES EXCLUSIVE RIGHT TO CORRECT THE PROBLEMS AND COLLECT ASSOCIATED COSTS FROM THE PERFORMANCE BOND.

WORK REQUIREMENTS

THE APPLICANT SHALL HAVE SUFFICIENT BARRICADES, WARNING SIGNS, AND LIGHTS DURING THE ENTIRE PERIOD THAT WORK IS BEING PERFORMED AND SHALL ADHERE TO APPLICABLE SECTION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

ALL DISTURBED AREAS MUST BE RETURNED TO A CONDITION THAT IS AS GOOD AS OR BETTER THAN THE CONDITION BEFORE THE WORK BEGAN. ALL REPAIRS MUST MEET CITY SPECIFICATIONS. THE CITY STREET SUPERINTENDENT WILL INSPECT AND APPROVE ALL REPAIRS. THE BOND WILL BE RETURNED AFTER ALL REPAIRS ARE APPROVED, IF APPLICABLE. FOR CLOSURE OF ARTERIALS OR BUSY COLLECTORS THE CITY RESERVES THE RIGHT TO DIRECT CONTRACTOR TO CLOSE THE STREET DURING OFF-PEAK TRAFFIC HOURS. CLOSURE MAY OCCUR AT NIGHT OR ON WEEKENDS. CONTRACTOR SHALL PROVIDE ALL TRAFFIC CONTROL ASSOCIATED WITH ROAD CLOSURE. EFFORTS SHALL BE MADE TO MINIMIZE ANY DISTURBANCE TO TREES OR ROOTS. EXCAVATION CAUSING DAMAGE TO TREES WILL RESULT IN THE REMOVAL AND REPLACEMENT BY THE CONTRACTOR. WHEN WORK ON RIGHT-OF-WAY OPENINGS RESTRICTS ACCESS TO PRIVATE PROPERTIES, OR OTHERWISE HAS A DIRECT IMPACT ON PRIVATE PROPERTIES IN THE OPINION OF THE ENGINEER, THE APPLICANT MUST NOTIFY ALL AFFECTED PROPERTY OWNERS IN WRITING, AND PROVIDE A COPY OF THE NOTIFICATION AND A MAILING LIST, PRIOR TO THE ENGINEERING DEPARTMENT ISSUING A PERMIT.

ALL CONTRACTORS WHO PERFORM WORK REQUIRING ENTRY INTO ANY CONFINED SPACE OF A CITY-OWNED UTILITY SHALL COMPLY WITH THE CITY'S CONFINED SPACE ENTRY PROCEDURES AND IN ACCORDANCE WITH ALL OSHA REGULATIONS, IF APPLICABLE, AND APPROVAL FROM THE WASTEWATER SUPERINTENDENT OR DESIGNEE.

MATERIAL SPECIFICATION

ALL WORK SHALL BE IN ACCORDANCE WITH THE ATTACHED DRAWINGS AND SPECIFICATIONS AND APPROVED BY THE PUBLIC WORKS DEPARTMENT PRIOR TO COMMENCEMENT OF WORK.

STREET OPENINGS - THE MATERIAL USED TO FILL IN A DITCH OR HOLE SHALL BE ODOT ITEM 603 GRANULAR MATERIAL (# 304, # 411) OR ODOT ITEM 613 LOW STRENGTH MORTAR BACKFILL. OTHER APPROVED GRANULAR MATERIALS MAY BE USED ONLY UPON THE CONTRACTOR RECEIVING PRIOR WRITTEN APPROVAL FROM THE PUBLIC WORKS DEPARTMENT IF EXTENUATING CIRCUMSTANCES EXIST. CONTRACTORS CONCERNED WITH THE CONTACT OF LOW STRENGTH MORTAR BACKFILL WITH THE FITTINGS AND THE PIPE MAY PLACE

A MAXIMUM OF 12 INCHES OF ODOT TYPE 603 BACKFILL ABOVE THE TOP OF A PIPE.

FOR RIGHT-OF-WAY OPENINGS BEYOND THE LIMITS OF THE PAVEMENT THE BACKFILL SHALL BE IN ACCORDANCE WITH THE ATTACHED DRAWINGS AND SPECIFICATIONS. ASPHALT SURFACE - ASPHALT SURFACE SHALL BE PLACED TO A DEPTH AS STATED IN ITEM 3, PAVEMENT RESTORATION, AS DETAILED IN THE ATTACHED SPECIFICATIONS.

CONSTRUCTION

REPAIR AREAS SHALL BE RECTANGULAR IN SHAPE WITH DIMENSIONS AS REQUIRED TO ENVELOP SURFACE DETERIORATION. AT THE DIRECTION OF THE CITY THE LIMITS OF THE REPAIRED PAVEMENT MAY BE EXTENDED AS DEEMED NECESSARY. PAVEMENT SHALL BE REMOVED BY METHODS THAT WILL NOT DAMAGE ADJACENT PAVEMENT.

ALL JOINTS AND VERTICAL FACES SHALL BE SAW CUT, CLEANED AND COATED WITH BITUMINOUS MATERIAL (TACK COAT) PRIOR TO PLACEMENT OF BITUMINOUS CONCRETE.

ALL JOINTS SHALL BE SEALED WITH AN ASPHALT SEALER AFTER THE FINAL SURFACE MATERIAL IS PLACED.

IF LOW STRENGTH MORTAR BACKFILL IS USED, THE LOW STRENGTH MORTAR BACKFILL SHALL BE BROUGHT UP UNIFORMLY TO THE FILL LINE SHOWN ON THE PLANS OR TO THE BOTTOM OF THE EXISTING PAVEMENT.

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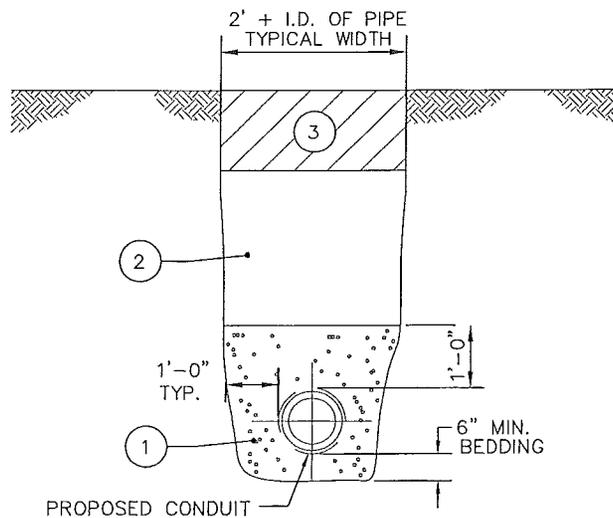
STREET CUT AND RIGHT-OF-WAY OPENING STANDARD DRAWINGS AND SPECIFICATIONS

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TRENCH DETAIL

BEDDING

GRANULAR BEDDING MATERIAL SHALL BE CRUSHED STONE OR GRAVEL COMPLYING WITH ODOT ITEM 603 TYPE 2 BEDDING (#57 OR # 67). BEDDING SHALL EXTEND 6 INCHES BELOW THE CONDUIT. BEDDING MATERIAL SHALL EXTEND 12 INCHES ABOVE THE TOP AND TO EACH SIDE OF THE CONDUIT. USE SHOVEL SLICING AND SPUD BARS IN CONJUNCTION WITH THE COMPACTION OPERATIONS TO COMPACT THE MATERIAL AND TO MANIPULATE THE MATERIAL UNDER THE HAUNCH OF THE PIPE.

BACKFILL

ALL TRENCH EDGES WITHIN THE STREET RIGHT-OF-WAY, UNDER OR WITHIN 5 FEET OF PROPOSED OR EXISTING PAVEMENT, CURB, DRIVEWAYS, ALLEYS, OR WALKS SHALL BE BACKFILLED WITH EITHER ODOT ITEM 603 GRANULAR BACKFILL MATERIAL (# 304, # 411) OR ODOT ITEM 613 LOW STRENGTH MORTAR BACKFILL.

- GRANULAR MATERIAL SHALL BE PLACED IN MAXIMUM 8-INCH LIFTS. FOR GRANULAR EMBANKMENT AND STRUCTURAL BACKFILL, COMPACT EACH LIFT OF MATERIAL USING MECHANICAL DEVICES, HOE RAMS, JUMPING JACKS, HAND DEVICES, VIBRATING PLATES, OR OTHER SIMILAR EQUIPMENT. COMPACTION REQUIREMENTS SHALL BE 98% OF STANDARD PROCTOR CURVE.
- LOW STRENGTH MORTAR BACKFILL SHALL BE FURNISHED AND PLACED AS PER ODOT ITEM 613.

ALL TRENCH EDGES NOT WITHIN THE STREET RIGHT-OF-WAY, NOT UNDER OR WITHIN 5 FEET OF PROPOSED OR EXISTING PAVEMENT, CURB, DRIVEWAYS, ALLEYS, OR WALKS CAN BE BACKFILLED WITH CLEAN NATIVE MATERIAL COMPACTED IN 12 INCH LIFTS. MATERIAL SHALL BE COMPACTED TO 85% OF THE ORIGINAL COMPACTION. NO MATERIAL SHALL BE USED FOR BACKFILLING THAT CONTAINS GRANULAR MATERIAL, ROCK OR STONE GREATER THAN 4 INCHES IN DIAMETER.

PAVEMENT RESTORATION

IN PAVED AREAS WITHIN THE STREET RIGHT-OF-WAY THE PAVEMENT AND AGGREGATE BASE COMPOSITION SHALL BE PROVIDED EQUAL TO THE EXISTING PAVEMENT BUT IN NO CASE SHALL THE COMPOSITION BE LESS THAN THE FOLLOWING:

- 1-1/2 INCHES OF ODOT ITEM 448 SURFACE COURSE, TYPE 1
- 2-1/2 INCHES OF ODOT ITEM 448 INTERMEDIATE COURSE, TYPE 2
- 10 INCHES OF ODOT ITEM 304 AGGREGATE BASE

IN ALLEYWAYS AND DRIVEWAYS OUTSIDE OF THE STREET RIGHT-OF-WAY THE REPLACEMENT OF PAVEMENT AND/OR AGGREGATE SHALL BE EQUAL TO THE EXISTING ALLEYWAY OR DRIVEWAY COMPOSITION. IF THE PERMANENT ASPHALT CANNOT BE APPLIED WITHIN 48 HOURS OF THE INITIAL REPAIR. ODOT ITEM 405 OR COLD PATCH SHALL BE APPLIED TO THE TRENCH SURFACE. MINIMUM THICKNESS OF THE COLD PATCH MATERIAL SHALL BE 3 INCHES. SAID MATERIAL SHALL BE REMOVED PRIOR TO THE PLACEMENT OF ODOT ITEM 448.

CONCRETE RESTORATION

ALL CONCRETE DRIVEWAYS, DRIVE APPROACHES, AND SIDEWALKS WITHIN THE STREET RIGHT-OF-WAY, SHALL BE REPLACED WITH THE FOLLOWING:

SIDEWALKS -- MINIMUM THICKNESS OF 4" INCHES OF CONCRETE

- MATCH ORIGINAL WIDTH OF SIDEWALK
- MINIMUM WIDTH OF 4 FEET REQUIRED
- DRIVEWAYS AND DRIVE APPROACHES:
- RESIDENTIAL: MINIMUM THICKNESS OF 6 INCHES OF CONCRETE
- BUSINESS: MINIMUM THICKNESS OF 8 INCHES OF CONCRETE
- INDUSTRIAL: MINIMUM OF THICKNESS 10 INCHES OF CONCRETE

ALL CONCRETE DRIVEWAYS, DRIVE APPROACHES, AND SIDEWALKS OUTSIDE OF THE STREET RIGHT-OF-WAY SHALL BE REPLACED EQUAL TO THE EXISTING MATERIAL COMPOSITION.

COMPACTION GUIDELINES

THE CONTRACTOR MAY OPERATE SMALL COMPACTION EQUIPMENT WITH LESS THAN A TOTAL WEIGHT OF 1 TON OVER THE CONDUIT TO COMPACT THE BACKFILL. DO NOT USE HOE RAMS ON TOP OF THE CONDUIT UNTIL 2 FEET OF BACKFILL IS COMPACTED ON TOP OF THE CONDUIT. THE CONTRACTOR MAY OPERATE COMPACTION EQUIPMENT WITH LESS THAN A TOTAL WEIGHT OF 8 TONS, BUT MORE THAN 1 TON, OVER THE CONDUIT AFTER PLACING AND COMPACTING 2 FEET OF BACKFILL. DO NOT OPERATE EQUIPMENT WITH A TOTAL WEIGHT OF 8 TONS OR MORE UNTIL PLACING AND COMPACTING A COVER OF 4 FEET OVER THE TOP OF THE CONDUIT. THE ABOVE RESTRICTIONS APPLY WHEN WORKING WITHIN ONE SPAN ON EACH SIDE OF THE CONDUIT, OR 6 FEET, WHICHEVER IS LESS.

ALL TRENCHES AND EXCAVATIONS SHALL BE BACKFILLED IMMEDIATELY AFTER THE PLACEMENT OF THE CONDUIT, UNLESS DIRECTED OTHERWISE BY THE CITY ENGINEER. UNDER NO CIRCUMSTANCES SHALL WATER BE PERMITTED TO RISE IN UNBACKFILLED TRENCHES AFTER THE CONDUIT HAS BEEN PLACED.

**AGGREGATE BASE, ASPHALT,
CONCRETE AND SUBGRADE TESTING**

AN INDEPENDENT CERTIFIED TESTING LABORATORY ACCEPTABLE TO THE CITY OF PIQUA IS TO BE USED BY THE CONTRACTOR TO PERFORM THE REQUIRED TESTING. THE TESTING LAB SHALL PROVIDE THE CITY OF PIQUA WITH ONE COPY OF ALL TEST RESULTS, INSPECTION LOGS ETC.

1) ASPHALT – ONE TEST SAMPLE PER TYPE, PER PAVED LAYER OR PAVER PASS, PER DAY.

a) TEST – GRADATION AND EXTRACTION TO DETERMINE BITUMEN CONTENT BASED ON SUBMITTED JOB MIX FORMULA.

b) THE ENGINEER MAY REQUIRE ADDITIONAL ASPHALT SAMPLES BY TYPE, PER PAVED LAYER OR PAVER PASS.

2) CONCRETE

- a) CURBING – ONCE DAILY DURING PLACEMENT
- b) DRIVEWAY – ONCE DAILY DURING PLACEMENT
- c) SIDEWALK – ONCE DAILY DURING PLACEMENT
- d) TESTING REQUIRED

- i) SLUMP TEST
- ii) AIR ENTRAINMENT
- iii) CONCRETE TEST CYLINDERS FOR

COMPRESSION STRENGTH TESTING AT 7 DAYS, 14 DAYS, AND 28 DAYS

e) THE ENGINEER MAY REQUIRE ADDITIONAL SAMPLES OF CONCRETE FOR CURB, DRIVEWAY OR SIDEWALK.

3) AGGREGATE BASE AND STREET SUB-GRADE

a) STREET SUB-GRADE

i) COMPACTION / DENSITY TESTING USING "ONE POINT" PROCTOR" AT 98%

ii) A MAXIMUM OF 100-FOOT INTERVALS OUTSIDE OF ALL UTILITY TRENCHES IN UNDISTURBED SOILS, ALTERNATING SIDES OF THE STREET.

iii) A MINIMUM OF 20 FEET FROM START AND END OF WORK AREA.

iv) A MINIMUM OF 3 FEET FROM THE PROPOSED CURB-LINE.

b) AGGREGATE BASE

i) COMPACTION / DENSITY TESTING USING STANDARD MATERIAL PROCTOR AT 98%

ii) A MINIMUM OF 20 FEET FROM START AND END OF WORK AREA.

iii) A MAXIMUM OF 100-FOOT INTERVALS ALTERNATING SIDES OF THE STREET.

iv) A MINIMUM OF 3 FEET FROM THE PROPOSED CURB-LINE.

c) THE ENGINEER MAY REQUIRE ADDITIONAL COMPACTION / DENSITY TESTING IN INTERSECTIONS, FILL AREAS OR IN AREAS OF UNDERCUTTING.

4) DRIVEWAYS AND SIDEWALK AREAS WHERE EMBANKMENT HAS BEEN PLACED OR UNDERCUTTING HAS OCCURRED AT THE DISCRETION OF THE ENGINEER.

a) COMPACTION / DENSITY TESTING USING STANDARD MATERIAL PROCTOR AT 98%

b) SIDEWALKS, AS DIRECTED

i) A MINIMUM OF 10 FEET FROM START AND END OF WORK AREA.

ii) A MAXIMUM OF 100-FOOT INTERVALS

c) DRIVEWAYS, AS DIRECTED

i) ONE PER AREA

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**AGGREGATE BASE, ASPHALT,
CONCRETE AND SUBGRADE TESTING**

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PERMIT TO BLOCK WAY

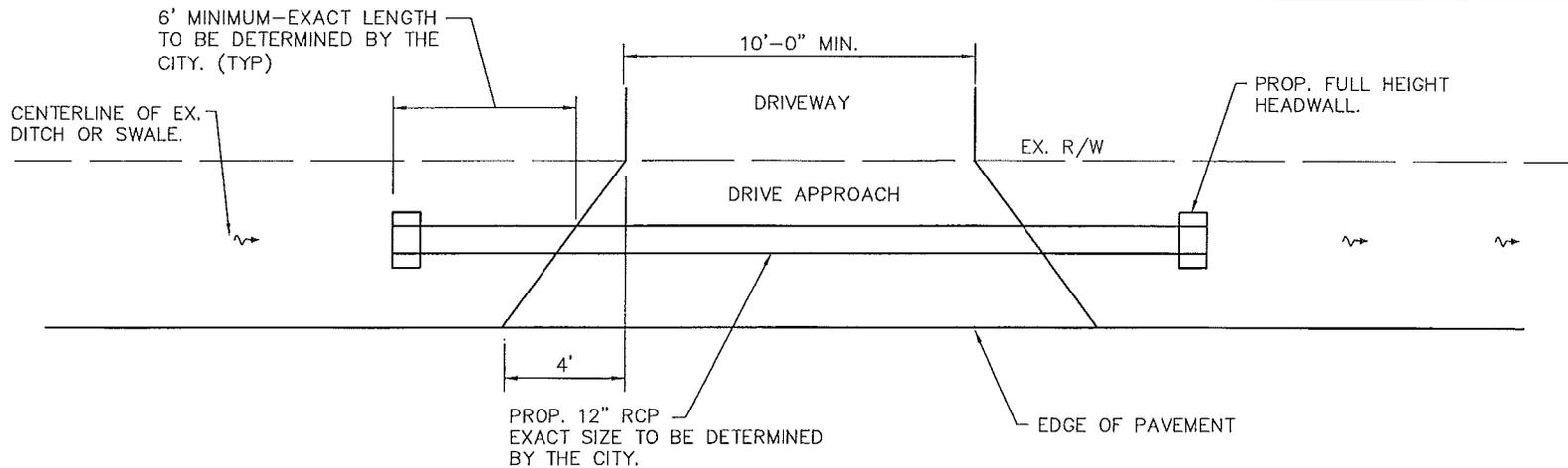
1. NO STREET, ALLEY, PARKING LANE, OR SIDEWALK SHALL BE BLOCKED WITHOUT THE CITY FIRST APPROVING A PERMIT TO BLOCK WAY. APPLICATIONS FOR A PERMIT TO BLOCK WAY CAN BE OBTAINED AT THE CITY OF PIQUA ENGINEERING DEPARTMENT OR ONLINE AT THE CITY OF PIQUA WEBSITE [WWW.PIQUAOH.ORG](http://www.piquaoh.org) <[HTTP://WWW.PIQUAOH.ORG](http://www.piquaoh.org)> E-GOV SERVICES, ONLINE DOCUMENTS.
2. APPLICATIONS FOR A PERMIT TO BLOCK WAY SHALL BE SUBMITTED TO THE CITY OF PIQUA ENGINEERING DEPARTMENT A MINIMUM OF 3 WORKING DAYS PRIOR TO THE REQUESTED BLOCKAGE.
3. APPLICATION SHALL INCLUDE A DETAILED DRAWING OF THE WORK ZONE LAYOUT AND INCLUDE SIGNAGE, CONES, BARRICADES, BARRELS ETC. ALL WORK ZONES SHALL CONFORM TO THE CURRENT EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
4. IT SHALL BE THE APPLICANT/CONTRACTORS RESPONSIBILITY FOR PROVIDING AND MAINTAINING ALL NECESSARY SAFETY MATERIALS FOR THE SET UP OF THE WORK ZONE.
5. THE CITY OF PIQUA ENGINEERING DEPARTMENT SHALL INSPECT THE WORK ZONE PERIODICALLY TO ASSURE THE MAINTENANCE OF THE DEVICES.
6. ALL EXCAVATION / REPAIR SHALL COMPLY WITH RIGHT-OF-WAY OPENING PERMIT REQUIREMENTS.

3. APPLICANT SHALL BE RESPONSIBLE FOR NOTIFYING IN WRITING ALL ADJACENT PROPERTY OWNERS TO BE AFFECTED BY THE CLOSURE. THE CITY SHALL RECEIVE A COPY OF THE WRITTEN NOTICE AND A LIST OF THE PROPERTY OWNERS AND THEIR ADDRESS THAT HAVE BEEN NOTIFIED.
4. APPLICATION SHALL INCLUDE A DETAILED DRAWING OF THE WORK ZONE LAYOUT AND INCLUDE SIGNAGE, CONES, BARRICADES, BARRELS ETC. ALL WORK ZONES SHALL CONFORM TO THE CURRENT EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
5. IT SHALL BE THE APPLICANT/CONTRACTORS RESPONSIBILITY FOR PROVIDING AND MAINTAINING ALL NECESSARY SAFETY MATERIALS FOR THE SET UP OF THE WORK ZONE.
6. THE CITY OF PIQUA ENGINEERING DEPARTMENT SHALL INSPECT THE WORK ZONE PERIODICALLY TO ASSURE THE MAINTENANCE OF THE DEVICES.
7. ALL EXCAVATION / REPAIR SHALL COMPLY WITH RIGHT-OF-WAY OPENING PERMIT REQUIREMENTS.

STREET OR ALLEY CLOSING PERMIT

1. NO STREET, ALLEY OR PARKING LANE SHALL BE CLOSED TO THRU TRAFFIC WITHOUT THE CITY FIRST APPROVING A STREET OR ALLEY CLOSING PERMIT. APPLICATIONS FOR A STREET OR ALLEY CLOSING PERMIT CAN BE OBTAINED AT THE CITY OF PIQUA ENGINEERING DEPARTMENT OR ONLINE AT THE CITY OF PIQUA WEBSITE [WWW.PIQUAOH.ORG](http://www.piquaoh.org) <[HTTP://WWW.PIQUAOH.ORG](http://www.piquaoh.org)> E-GOV SERVICES, ONLINE DOCUMENTS.
2. APPLICATIONS FOR A STREET OR ALLEY CLOSING PERMIT SHALL BE SUBMITTED TO THE CITY OF PIQUA ENGINEERING DEPARTMENT A MINIMUM OF 5 WORKING DAYS PRIOR TO THE REQUESTED BLOCKAGE, EXCLUDING WEEKENDS AND HOLIDAYS.

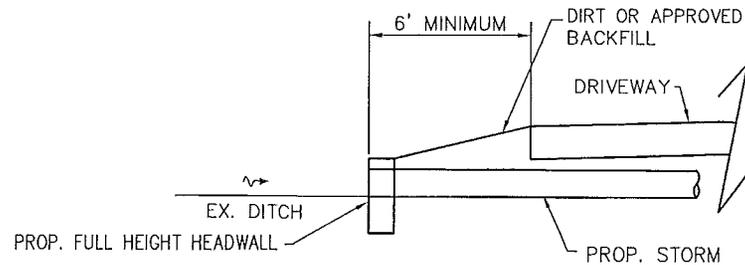
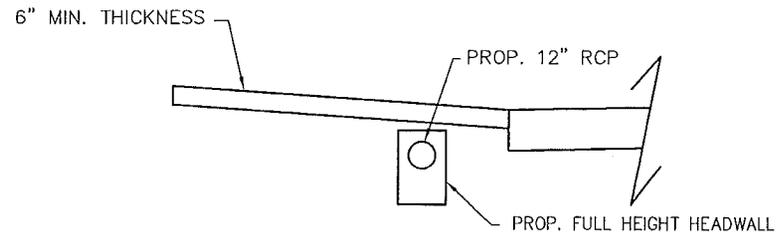
CITY OF PIQUA	STREET CLOSING AND BLOCK WAY PERMITS	REVISIONS:	DATE APPROVED: AUG. 2008
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DRIVE WITH CULVERT

NOTES

- A. DRIVE APPROACHES SHALL BE CONSTRUCTED PER THE CITY STANDARDS AND APPROVAL.
- B. ALL NEW CONSTRUCTION OR MODIFICATIONS OF DRIVE APPROACHES REQUIRE A CONCRETE APPROACH, REGARDLESS OF WHETHER THERE IS A SIDEWALK OR NOT. THE NEW APPROACH IS TO GO FROM EDGE OF EXISTING STREET TO RIGHT OF WAY OR A MINIMUM OF 6'-0".
- C. ANY DRAINAGE ISSUES WILL HAVE TO BE ADDRESSED, WHEN A DRIVEWAY IS INSTALLED OR MODIFIED.
- D. IF THE EXISTING ROADWAY IS DISTURBED DURING CONSTRUCTION THE PROPERTY OWNER WILL BE RESPONSIBLE TO REPAIR ANY DAMAGE TO THE SATISFACTION OF THE CITY.
- E. THE PROPERTY OWNER MUST SUMMIT A DRAWING TO BE APPROVED BY THE CITY PRIOR TO CONSTRUCTION.
- F. THE EXACT SIZE AND LOCATION OF THE PROPOSED CULVERT AND HEADWALLS WILL BE DETERMINED BY THE CITY PRIOR TO CONSTRUCTION.



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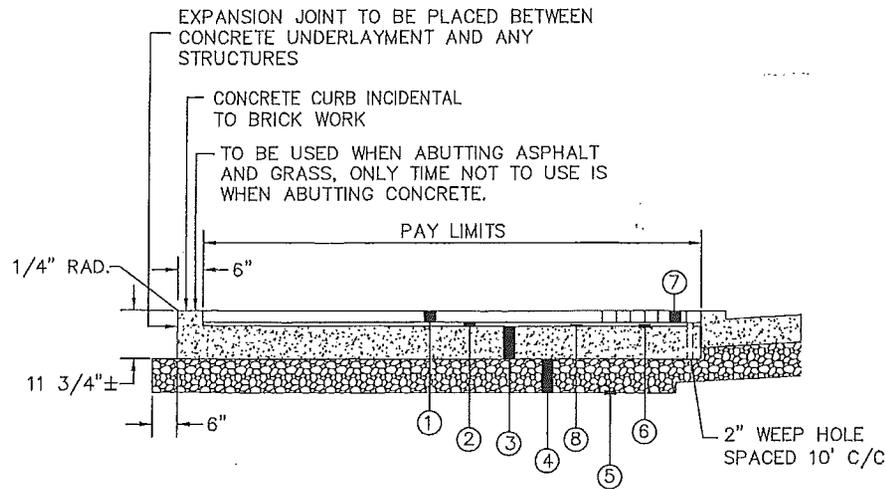
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DRIVE APPROACH WITH CULVERT

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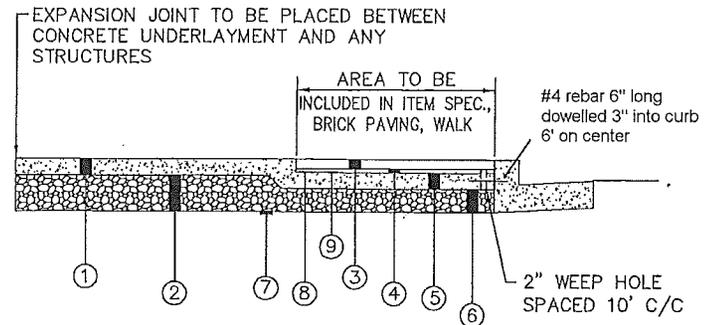
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- ① ITEM SPECIAL, BRICK PAVING, CURB RAMP (2-1/4" BRICK)
- ② 3/4" ROLLED BITUMINOUS SETTING BED. (REFER TO VILLAGE FOR SPECIFICATIONS)
- ③ ITEM 608, 8" CONCRETE UNDERLAYMENT, CURE & SEAL WITH ONE COAT OF SUPER DIAMOND CLEAR
- ④ ITEM 411, 8" AGGREGATE BASE SATURATE W/ WATER PRIOR TO COMPACTION
- ⑤ ITEM 204, SUBGRADE COMPACTION
- ⑥ ITEM 407, TACK COAT @ 0.10 GAL/S.Y.
- ⑦ TRUNCATED DOMES (2 1/4" BRICK)
- ⑧ NEOPRENE MODIFIED ASPHALT ADHESIVE.

BRICK TYPICAL SECTION AT CURB RAMPS

IF CURB IS REMOVED AND REPLACED DURING DRIVEWAY CONSTRUCTION, UNDERLAYMENT SHALL BE DOWELLED WITH #4 REBAR 6" INTO CURB AND GUTTER 18" O.C.



- ① ITEM 608, 5" CONCRETE WALK, AS PER PLAN APPLY 2 COATS OF SUPER DIAMOND CLEAR CURING AND SEALING COMPOUND, OR APPROVED EQUIVALENT PER MANUFACTURER'S RECOMMENDATION ON ALL SURFACES INCLUDING BACK IMMEDIATELY AFTER FINISHING SURFACES.
- ② ITEM 411, 8" AGGREGATE BASE, (IN 2 EQUAL 4" LIFTS), SATURATED W/ WATER PRIOR TO COMPACTION
- ③ ITEM SPECIAL, BRICK PAVING, WALK (2-1/4" BRICK), (REFER TO CITY FOR SPECIFICATIONS)
- ④ 3/4" ROLLED BITUMINOUS SETTING BED. (REFER TO VILLAGE FOR SPECIFICATIONS)
- ⑤ ITEM 608, 4" CONCRETE UNDERLAYMENT, CURE & SEAL WITH ONE COAT OF SUPER DIAMOND CLEAR.
- ⑥ ITEM 411, 5-3/4" AGGREGATE BASE SATURATE W/ WATER PRIOR TO COMPACTION
- ⑦ ITEM 204, SUBGRADE COMPACTION
- ⑧ ITEM 407, TACK COAT @ 0.10 GAL/S.Y. NEOPRENE MODIFIED ASPHALT ADHESIVE.
- ⑨ NEOPRENE MODIFIED ASPHALT ADHESIVE.

BRICK TYPICAL SECTION

(REFER TO SPECIFICATIONS)

CITY OF PIQUA	STREETSCAPE BRICK WITH ASPHALT LAYING COURSE TYPICAL SECTIONS	REVISIONS: 04/10/14	DATE APPROVED: AUG. 2008
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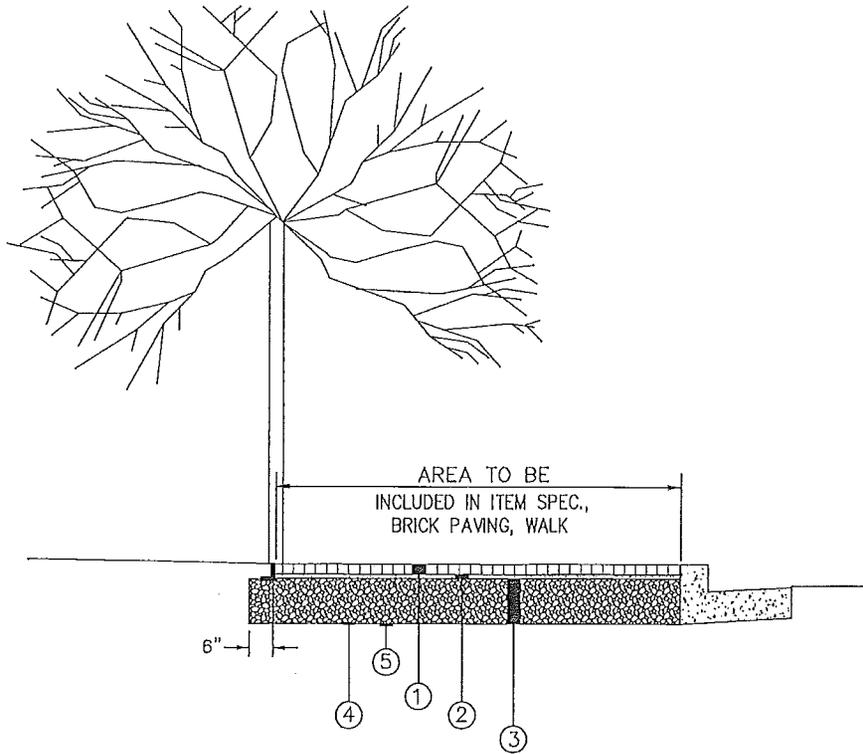
EXACT AREA TO BE DETERMINED BY THE EXISTING TREE DRIP LINE.

PAVER EDGE RESTRAINT SYSTEM, PAVE EDGE OR EQUIVALENT.

EX. TREE

12"

PROPOSED CONCRETE SIDEWALK



- ① ITEM SPECIAL, BRICK PAVING, WALK (2-1/4" BRICK), (REFER TO CITY FOR SPECIFICATIONS).
- ② 1 1/2" SAND LAYING COURSE. (REFER TO CITY FOR SPECIFICATIONS).
- ③ ITEM 411, AGGREGATE BASE, SATURATED W/ WATER PRIOR TO COMPACTION.
4" MINIMUM - PEDESTRIAN
8" MINIMUM - VEHICULAR (IN 2 EQUAL 4" LIFTS)
- ④ ITEM SPECIAL, GEOTEXTILE FABRIC (REFER TO CITY FOR SPECIFICATIONS).
- ⑤ ITEM 204, SUBGRADE COMPACTION

BRICK WITH SAND TYPICAL SECTION

CITY OF PIQUA

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STREETSCAPE BRICK WITH SAND LAYING COURSE TYPICAL SECTION

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MATERIALS:

A. BRICK PAVERS

1. MANUFACTURED FROM EXTRUDED FIRECLAY OR SHALE AND FIRE TO PRODUCE A DENSE PAVER THOROUGHLY ANNEALED AND EVENLY BURNED SO THAT, WHEN BROKEN, THEY SHOW A UNIFORMLY DENSE STRUCTURE FREE FROM LIME, AIR POCKETS, AND MARKED LAMINATIONS.
2. PROVIDE BRICK PAVERS IN WALK AREAS IN ACCORDANCE WITH ASTM C 902, CLASS SX, TYPE I.
3. PAVER THICKNESS SHALL BE 2-1/4" (MINIMUM).
4. PROVIDE A BEVEL-EDGE BRICK PAVER OF QUALITY AND COLOR TO MATCH THE TONE AND HUE OF THE BRICKS LISTED BELOW, AS APPROVED BY THE ENGINEER. PROVIDE THREE REPRESENTATIVE BRICKS FOR COLOR APPROVAL.

BRICK PAVER SPECIFICATIONS:

ENGLISH EDGE RF (RED FLASH) BRICK PAVER #552475

DIMENSIONS 2-1/4" X 4" X 8" AS MANUFACTURED BY PINE HALL BRICK FOR WALKS AND CURB RAMPS

BRICK MUST MEET DIMENSIONS AND QUALITY. CONTRACTOR IS TO DISCARD ANY DEFECTIVE BRICKS.

COLOR BLENDING INSTRUCTIONS:

PAVERS ARE TO BE LAID FROM MULTIPLE PALLETS TO ACCOMPLISH A UNIFORM BLENDING OF COLORS.

DETECTABLE WARNING PAVER SPECIFICATIONS:

COLOR IS TO BE BROWN TO MATCH COLOR USED IN PHASE II.

DIMENSIONS 2-1/4" X 4" X 8" AS MANUFACTURED BY PINE HALL BRICKS FOR CURB RAMPS.

BRICKS MUST MEET DIMENSIONS AND QUALITY. CONTRACTOR TO DISCARD ANY DEFECTIVE BRICKS.

5. CLAY PAVERS SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 7,500 PSI WITH NO INDIVIDUAL STONE TESTING LESS THAN 7,200 PSI.
6. THE CONTRACTOR SHALL NOT INSTALL ANY BRICK PAVING MATERIAL INCLUDING CONCRETE UNDERLAYMENT UNTIL THE SHOP DRAWINGS HAVE BEEN APPROVED BY THE CITY.

B. INSTALLATION OF BRICK PAVERS

1. PLACE THE PAVERS BY HAND WHEN THE MODIFIED ASPHALT ADHESIVE IS DRY TO THE TOUCH. PLACE IN STRAIGHT COURSES, CONTROLLED BY ACCURATELY PLACED STRIKING LINES, WITH HAND-TIGHT JOINTS AND UNIFORM TOP SURFACE. KEEP GOOD ALIGNMENT AND THE PATTERN THAT IS SHOWN ON THE PLANS OR APPROVED SHOP DRAWINGS. ESTABLISH COURSING PERPENDICULAR TO STREET CENTERLINE.

2. PROTECT NEWLY LAID PAVERS AT ALL TIMES BY PANELS OF PLYWOOD ON WHICH THE INSTALLER STANDS. ADVANCE THESE PANELS OF PLYWOOD AS WORK PROGRESSES. KEEP THE PLYWOOD PROTECTION IN AREAS WHICH WILL BE SUBJECTED TO CONTINUED MOVEMENT OF MATERIALS AND EQUIPMENT.

A. JOINT TREATMENT

1. VARY HAND-TIGHT JOINTS FROM 0" TO A MAXIMUM 3/16" WITH 1/8" BEING THE AVERAGE JOINT DIMENSION. FILL JOINTS WITH STABILIZED JOINT FILLER CONSISTING OF SAND AND SANDLOCK MIXTURE. SEE 4 THROUGH 7 BELOW FOR INSTALLATION.

2. SAW BRICK PAVERS TO FIT TIGHTLY AROUND ALL OBSTACLES WITHIN THE PAVING AREA. MEET JOINTING TOLERANCE OF HAND-TIGHT JOINTS. FIT PAVERS AROUND CIRCULAR OBJECTS WITH CIRCULAR OR ARCH FORM, NOT ANGULAR.

3. SAW BRICK PAVERS TO FIT WITHIN THE PATTERN AROUND OBSTACLES AND PROVIDE A SURFACE AREA GREATER THAN ONE-THIRD A FULL-SIZE BRICK, UNLESS APPROVED BY THE CITY AS THE ONLY ALTERNATIVE.

4. BLENDING SANDLOCK WITH SAND

- A. UNIFORMLY BLEND 1-1/2 LBS. OF SANDLOCK PER 100 LBS. OF DRY JOINT SAND MATERIAL. ON-SITE MIXING RATIO MAY BE ADJUSTED BETWEEN 1 AND 1-3/4 LBS. PER 100 LBS. OF SAND.

- B. BLENDING CAN BE ACCOMPLISHED EITHER BY HAND MIXING OR DRY MIXING IN A MECHANICAL MIXER ON SITE, OR MATERIAL CAN BE PRE-MIXED IN PROPER PROPORTIONS AND DELIVERED TO THE WORK SITE. MIX ONLY ENOUGH MATERIAL TO COMPLETELY FILL THE JOINTS OF THE PAVERS WHICH HAVE BEEN SET THAT DAY. ONCE MIXED, MATERIAL MUST REMAIN COMPLETELY DRY PRIOR TO APPLICATION; PROPER PRECAUTION SHOULD BE TAKEN TO PROTECT MATERIALS FROM EXCESSIVE MOISTURE EXPOSURE PRIOR TO USE. WHILE SANDLOCK CAN BE ADDED TO DAMP SAND, IT SHOULD NOT BE USED WITH SAND THAT IS WET. BE SURE TO COVER SAND PILES AT THE END OF EVERY WORK DAY OR IF RAIN IS

5. PLACEMENT OF MIXED JOINT MATERIAL

SPREAD MIXED JOINT SAND MATERIAL OVER PAVED SURFACE EVENLY; COMPLETELY COVER PAVER SURFACE WITH THIN LAYER OF MIXED AGGREGATE AND SANDLOCK. USING A PUSH BROOM SWEEP THE MIXTURE INTO THE JOINTS WITH A SLIGHT POUNDING MOTION. CONTINUE THIS PROCEDURE UNTIL ALL JOINTS ARE FULL AND SAND CAN NO LONGER BE VIBRATED INTO THEM.

6. ACTIVATION OF BONDING

A. CAREFULLY SWEEP ENTIRE PAVEMENT CLEAN TO REMOVE SANDLOCK MIXTURE FROM THE PAVER SURFACES. EXCESS SAND MATERIALS INCLUDING CHAMFERED AREAS MUST BE SWEEP OFF PAVED SURFACE AND REMOVED.

POWER BROOMS OR BLOWERS ARE RECOMMENDED FOR LARGE AREAS. EXCESS MATERIAL REMAINING ON SURFACE AFTER THE MIXTURE HAS BEEN ACTIVATED IS DIFFICULT TO REMOVE.

B. THE PAVED AREA, INCLUDING JOINTS, SHOULD BE FLOODED WITH WATER TO ACTIVATE THE SANDLOCK ADDITIVE. CARE MUST BE TAKEN TO AVOID

WASHING SAND FROM THE JOINTS. DO NOT USE HIGH PRESSURE SPRAY FOR FLOODING SURFACE. AFTER FLOODING, AREA SHOULD BE COMPLETELY DRY, INCLUDING JOINTS, PRIOR TO PERMITTING TRAFFIC TO USE PAVED AREAS. DRYING TYPICALLY OCCURS WITHIN 2 TO 6 HOURS, DEPENDING UPON WEATHER CONDITIONS.

7. CLEANUP

IF SANDLOCK IS LEFT ON SURFACE, IT WILL EMULSIFY. DEPENDING ON SEVERITY, IT MAY COME OFF WITH WATER AND BRUSHING; IF SEVERE, IT MAY REQUIRE PRESSURE WASHING. NEVER LEAVE DRY SANDLOCK RESIDUE ON SURFACE!

<p>CITY OF PIQUA</p>	<h1>STREETSCAPE BRICK SPECIFICATIONS</h1>	<p>REVISIONS:</p>	<p>DATE APPROVED: AUG. 2008</p>
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